

Birmingham Inland Border Facility Reinstatement Plan

Town and Country Planning (Border Facilities and Infrastructure)
(EU Exit) (England) Special Development Order 2020

Her Majesty's Revenue and Customs
Gleeds Building Surveying Ltd
NTB3728

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1

Introduction

1.0 Introduction

Gleeds Building Surveying Limited have been appointed by Her Majesty's Revenue and Customs (HMRC) to produce a Reinstatement Plan for the Birmingham Inland Border Facility (IBF). The Reinstatement Plan is a requirement of the Town and Country Planning (Border Facilities and Infrastructure) (EU Exit) (England) Special Development Orders of 2019 and 2020 (SDO) and this must be approved by the Secretary of State for the Department for Levelling Up, Housing and Communities (formally the Ministry of Housing, Communities and Local Government (MHCLG)) prior to any reinstatement work on this site. The definition of 'reinstatement works' in the SDO means 'the cessation of the use, and removal of all buildings, for which planning permission is granted by article 3(1)(a) and (b) on a site and the restoration of the site to its condition before such development commenced, except as specified in a reinstatement plan approved by the Secretary of State'.

This report has been produced for the benefit of The Department for Levelling Up, Housing and Communities (DLUHC) to determine the state of Birmingham Inland Border Facility (IBF) following cessation of IBF operation, reinstatement, and handover.

Approval was granted via a Special Development order on 20 November 2020 by the then MHCLG and is known as Town and Country Planning (Border Facilities and Infrastructure) (EU Exit) Special Development Order 2020 which includes the site known as Birmingham Airport, Car Park 6, Jetstream Road, B26 3QY. The approval provided consent for a temporary development comprising the change of use of the site from an existing car park to provide HGV and staff car parking, creation of temporary structures, installation of lighting columns and other minor alterations to suit site operations.

This report relates to the area occupied by HMRC and as shown on the red line plan contained within Appendix A. This Reinstatement Plan defines the planned reinstatement work at Birmingham IBF with commentary and drawings identifying where and what work is required to reinstate the site to its former condition prior to the 2020 SDO and where HMRC intend to retain site features and the justification for doing so.

A general summary of those items to be removed are:

- Temporary modular accommodation
- Temporary inspections sheds
- Temporary gatehouses
- Vehicle access barriers and turnstiles
- Concrete barriers
- Site Signage (fixed, temporary and adhesive)
- Signage to Strategic and Local Road Network

- Generators
- Site wide lighting
- Site wide CCTV
- Feeder pillars
- Perimeter electrical distribution
- Armco barriers
- Foul water holding tanks including below ground foul water drainage system
- Cycle and smoking shelters
- Water storage containers (bulk)
- Spare lamp standards
- Inspection equipment (loose)
- Any concrete base, foundation or slab to accommodation or fitting
- Emergency pedestrian boundary gate

A general summary of those items to be reinstated are:

- Site wide lighting (to modern design which may involve reduced fittings and layout)
- Tarmac surfacing in accordance with lease provision
- Site entrance kerbing, arrangement and bus shelter
- Paving and subbase where accommodation installed
- Paving and subbase to where generator base cast into slope
- Damaged perimeter kerbs
- White lining to parking bays

A general summary of those items to be retained are:

- Adaptions to perimeter boundary security
- Adaptions to surface water drainage system

2

Description of Reinstatement Work

2.0 Description of Reinstatement Work

2.1 Introduction

The purpose of this section of the report is to provide a clear description of the site prior to occupation, the proposed reinstatement work and the target condition of the site following the reinstatement.

2.2 Site Description Prior to Occupation

Prior to occupation, the site was known as Birmingham International Airport Car Park 6 and was originally constructed for use as a car park to service the adjacent Airport. The car park was underutilised and as such prior to HMRC occupation the site was recently used as an overflow car park for Jaguar Land Rover whilst redevelopment works took place at their nearby Lode Lane factory site.

The site extends to approximately 4.7 hectares and is bounded by the A45 to the South, car parks to the North and West and a commercial development to the East. The site previously comprised of predominantly tarmac hardstanding with trees equally spaced out between car parking spaces across the whole of the site. The site is bound by a weldmesh fence with vegetation including trees set both immediately inside and outside of the boundary line. There were two demountable buildings on site as well as a bus shelter off the main access road (Jetstream Road). There was a taxi depot immediately adjacent to the entrance to the car park which is no longer occupied.



Site Location Map

2.3 Description of Reinstatement Work

The reinstatement work is intended to revert the site back to its state prior to HMRC occupation with some beneficial features being retained which are described and justified in Section 3 of this document.

2.3.1 Temporary Accommodation

The temporary accommodation installed to enable the site to operate is all to be removed and surfaces disturbed as a result of installation will be made good. These units are listed below:

Item	Reason for removal
Examination Facilities	Temporary accommodation
Examination Offices	Temporary accommodation
Border Force Office	Temporary accommodation
Marshals Facilities	Temporary accommodation
On Site Facilities Office	Temporary accommodation
Marshals Tent	Temporary accommodation
Drivers Facilities	Temporary accommodation
Control Building	Temporary accommodation
Generators and Assoc. Equipment	Installed for IBF site operation, no longer required
Shipping Containers	Temporary accommodation
Smoking Shelters	Temporary accommodation
Cycle Shelter	Temporary accommodation
Access Ramps and Stairs	Temporary accommodation
Spill Kit Shelters	Temporary accommodation
Security Cabins	Temporary accommodation
Raised Gate Booths	Installed for IBF site operation, no longer required
Water and Waste Tanks	Installed for IBF site operation, no longer required
Concrete Barriers	Installed for IBF site operation, no longer required

2.3.2 Services

Adapted services to serve the IBF site mainly included the external lighting with some minor adaptations to the incoming electrical services to relocate the feeder pillar box serving the site. Site facilities and temporary accommodation are predominantly serviced by the on site generators via above an above ground cable network with associated feeder pillars to the perimeter of the site, with some cabling situated underground to facilitate site access and vehicle movements, all of which will be removed as part of the reinstatement works.

New lighting columns that were installed in accordance with the SDO will be removed, however it is intended to re-use lighting columns to suit the existing site where possible including the modern type LED fittings which may result in a reduced number of fittings and layout. Further details on this can be found in Section 3.0.

The on site CCTV network including columns and equipment will be removed from site and disturbed areas reinstated. The external CCTV traffic monitoring cameras installed will be retained by the council.

As foul water connections were not available to this site, a large partially above ground concrete constructed Bio-digester tank for which final discharge to a local watercourse was subsequently not obtained. All holding tanks will be removed from site and existing surfaces made good including all below ground foul drainage pipework and manholes/inspection chambers.

Surface water drainage modifications to suit IBF site operations such as linear drainage and the like will be removed and areas reinstated. Other surface water modifications are proposed to be retained and are described further in Section 3.0.

The reinstatement of the services can be summarised as follows:

Item	Reason for removal
Perimeter above ground electrical distribution	Installed for IBF site operation, no longer required.
External Lighting Columns	Installed for IBF site operation.
CCTV Columns	Installed for IBF site operation, no longer required.
Foul Water Holding Tanks including below ground foul drainage pipework	Installed for IBF site operation, no longer required.
Surface Water Drainage	Partially reinstated back to original layout. Proposal to retain some elements for site benefit

2.3.3 Civil Engineering Works

The principal entrance (Jetstream Road) leading from the Airport Way roundabout was heavily modified to suit the IBF requirements including the removal of kerb lines, traffic barrier islands and a bus stop including bus shelter. This will all be reinstated to suit the original car park requirements, with consideration for some works to be retained as detailed in section 3.0. Other kerb lines that were installed to form car parking bays and internal access roads will also be reinstated.

The perimeter boundary security fencing remained largely in-situ, and slightly modified to suit the revised main entrance requirements which will be reinstated to suit the original car parking requirements. Again consideration for some works to be retained is described later in this report. Armco crash barriers situated along the inside face of the Western and Southern boundaries will be removed.

A timber fenced waste enclosure was constructed along the Northern boundary of the site and will also be removed, and the surfacing reinstated.

Existing tarmac surfaces disturbed by the installation of temporary structures, concrete slabs to support water tanks, cycle shelters, raised gate booths, as well as an excavated area in the North Western part of the site to provide a level surface for the installation of the temporary generators and associated equipment will all be made good prior to the complete resurfacing of all previous hardstanding areas in accordance with the yielding up provisions of the lease with Birmingham Airport Limited.

The adaptations to the site line marking will be removed and the original layout will be reinstated using thermoplastic material as part of the resurfacing works.

The reinstatement of the civil engineering works can be summarised as follows:

Item	Reason for removal
Principal Site Entrance Modifications	Installed for IBF site operation, no longer required. Not suitable for reinstated use as a car park. Some modifications proposed to be retained.
Timber fenced waste enclosure and Armco barriers	Installed for IBF site operation, no longer required.
Surfacing & Hardstanding Modification Modifications	Modified for IBF site operation, no longer required.
Line Marking	Reinstate to original layout.

2.4 Condition of Site at Conclusion of Reinstatement Work

The intended condition of the site following the reinstatement work will generally be as it was when HMRC first leased it. The exceptions to this are as detailed in Section 3 of this report and are namely:

1. Adaptions to perimeter boundary security – retention of lockable access gates to the main vehicular access into the site to provide full perimeter security. Retention of additional weld mesh fence installed to the adjacent taxi parking and accommodation area in lieu of the previously installed jersey barriers.
2. Adaptions to surface water drainage system – retention of forecourt interceptor and penstock valve added along Eastern Boundary of the site before discharging into the nearby watercourse.
3. Vegetation installed between car parking bays – proposal not to reinstate approximately 130 low quality trees and associated kerb lines (vegetation removed to site perimeter and to kerbed islands to be reinstated on a like for like basis).

3

Temporary Development to be
Retained

3.0 Temporary Development to be Retained

3.1 Introduction

The purpose of this section of the report is to explain and justify which elements of the temporary development are to be retained. This section of the report covers items that were approved under article 4(1) of the SDO.

3.2 Features to be Retained

3.2.1 Site Boundary Fencing

The perimeter site boundary fencing remained largely unchanged as part of the IBF development works, apart from the following:

- Adaptions to main vehicular access road (Jetstream Road) to provide a lockable weldmesh double weldmesh gates
- Additional weldmesh fencing to separate main IBF site from the adjacent taxi parking and accommodation
- Provision of pedestrian access gate along the Western Boundary of the site for the purposes of an alternative means of escape from the site.

Following initial discussions with the landlord Birmingham Airport Limited, it is proposed to retain the vehicle access gate and additional fencing to the taxi parking elements to provide additional physical security and future beneficial use of the site, however the pedestrian access gate will be reinstated back to boundary fencing.

3.2.2 Surface Water Drainage System

As part of the IBF development works and at the request of the Environment Agency under their permit conditions, an oil interceptor and penstock valve was installed on the existing surface water drainage system on the final drainage run before it discharges into the nearby water course. This was installed as a means to enable sufficient water quality to discharge into the existing watercourse, as well as fully isolate the drainage system on the site in the event of an emergency.

Given the sites existing use as a car park and in discussions with the landlord Birmingham Airport Limited, it is proposed that these two additional elements are retained to provide enhanced pollution prevention methods and subsequent discharge into the watercourse and in the downstream SSSI.

3.2.3 Vegetation to Car Parking Bays

To facilitate the IBF development, approximately 130 young to semi mature low quality (category C) trees were removed from the site. These were individually planted trees equally spaced between car parking bays across the site with associated kerb lines to prevent accidental damage from vehicles. It was noted prior to the development works that the trees were causing underlying tree root damage to the adjacent paving surfaces as well as fouling car parking spaces within close proximity of the trees thereby making these spaces difficult to use.

Following discussions with the landlord Birmingham Airport Limited and Solihull Metropolitan Borough Council it is proposed that these trees are not reinstated to their original locations, but support off site replacement similar to the request laid out during the stakeholder engagement process under the Article 4 (1) submission by Solihull Metropolitan Borough Council (SMBC) as part of their Urban Forestry Strategy and adopted Local Plan. HMRC has agreed to provide further funding (of up to £50,000 to include planting costs and maintenance) to their "Planting our future" initiative. This initiative seeks to plant 250,000 new trees in the borough and will run until 2030.

This would not be for like for like planting but would be for a larger number of young trees (anticipated to be at least 390 whips up to a maximum of 1000 depending on cost and space available at the time) of native species. The Council is currently seeking and reviewing their land options for planting for next year and this contribution will be used at the earliest opportunity (but within the next three years) once a suitable scheme has been identified.

It is noted that the vegetation partially removed to the site boundary and the kerbed islands forming the car park access road around the perimeter of the site is to be reinstated on a like for like basis.

4

Long Term SDO Approved Elements

4.0 Long Term SDO Approved Elements

4.1 Introduction

The purpose of this section of the report is to outline which, if any, of the physical elements of the Inland Border Facility (IBF) scheme approved by the SDO are intended for retention at the cessation of the IBF operation. This section of the report sets out the detail of these elements and provides justification for doing so, such as prior planning consents or where beneficial use outweighs removal.

4.2 Elements with SDO Approval for Retention

Not applicable

5

Article 4 Compliance

5.0 Article 4 Compliance

5.1 Introduction

The purpose of this section of the report is to outline where reinstatement follows that described in the Article 4 submission for the original SDO approval. In particular it identifies where reinstatement or retained features could give rise to significant environmental effects that were originally anticipated to be avoided. Where effects are envisaged they are assessed and justified.

5.2 Statement Detailing Article 4 Compliance

The following is based on the original Article 4 Analysis of Likely Environmental Effects of the Development (ALEED) submission which in each case predicted that there would be no likely significant effect. This is also the case with the planned reinstatement work. For specific references to the relevant lines of the REAC see Section 22 of this document.

Environmental Effect	Assessment and Justification
Air Quality	<p>The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There is the potential for the creation of dust particularly when reinstating or removing hardstanding areas. The implementation of best practice construction methods will be adopted to control dust such as</p> <ul style="list-style-type: none"> • ensuring that all vehicles with open loads of duty materials are securely sheeted or enclosed. • Minimising stockpile heights and locate out of the wind • Enforcement of speed limits on surfaced/unsurfaced roads • Damp down in dry conditions • Vehicle engines and plant motors switched off when not in use
Cultural Heritage	<p>The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. No new excavations are proposed that would potentially impact on any unknown archaeological remains. Temporary impacts to the setting of nearby heritage assets from reinstatement activities would be minor and the effects would not be significant. No permanent impacts on heritage assets are anticipated and as a result no additional mitigation measures are required.</p>

Landscape and Visual Effects	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. Vegetation removed from the perimeter of the site boundary to facilitate the security of the site and the installation of perimeter services will be reinstated on a like for like basis. It is proposed that the previous low quality trees situated between the car parking bays will be replanted off site to support SMBC's Local Plan.
Geology and Soils	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. All excavations will be in areas previously disturbed during the construction of the site so no significant effects are anticipated during reinstatement. However, best practice measures will be adopted during reinstatement including the provision of spill kits with reinstatement staff trained in their correct application.
Biodiversity	<p>The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. Best practice measures will be followed however as part of the works including:</p> <ul style="list-style-type: none"> • Any construction lighting required to be low or high pressure sodium lamps fitted as low as possible and use directional luminaries to direct light only where required so as not to disturb any existing species and habitats • Installation of protective barriers to existing protect root protection areas based on advice from a Arboriculturalist where appropriate • Vehicular movements on hardstanding only, as well as compound areas for reinstatement works
Material Assets and Waste	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There will be waste produced as part of the reinstatement works so in order to reduced any non-significant effects best practice measures such as the principles of the waste hierarchy are adhered to including the prevention, reuse and preparation for re-use, recycling, recover and disposal of waste. Opportunities will be explored for the temporary structures (offices and inspection sheds) to be sold and re-used elsewhere where they are not leased.
Noise and Vibration	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There is the potential for a temporary adverse effect on nearby sensitive receptors as a result of noise arising from the works associated with the reinstatement such as the dismantling of the temporary structures. As this is small scale in nature and of a temporary duration no significant effects are anticipated, however best practice measures to

	minimise noise and noise limits as set out in BS5228-1 will be followed by the reinstatement contractor.
Population and Human Health	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. No road or public rights of way closures are proposed and there would be no restrictions on local businesses as part of the reinstatement. There will be some slight disturbance from construction machinery and noise as part of reinstatement however it is proposed that the local community will be informed of the proposals for reinstatement including approximate timescales, as well as controlling HGV movements on site by appropriate site marshals.
Road Drainage and Water Environment	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There are potential effects to the water environment during reinstatement due to existing pollution pathways, however it is proposed that the penstock valve installed as part of the IBF would be retained and as such would help to reduce any potential. In addition, best practice guidance will be followed in line with guidance CIARA (2001).
Climate	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. Opportunities will be explored for the temporary structures (offices and inspection sheds) to be sold and re-used elsewhere where they are not leased.

It is therefore considered that no significant impacts will arise as a result of the reinstatement works over and above the original Article 4 submission.

6

Plans and Drawing References

6.0 Plans and Drawing References

6.1 Introduction

The purpose of this section of the plan is to detail the documentation describing the planned reinstatement work across the site.

6.2 Plans and Drawing References and Descriptions

Drawings can be found in Appendix A of this report.

7

Programme

7.0 Programme

7.1 Introduction

The purpose of this section of the report is to detail the programme for the reinstatement work which demonstrates that it will be completed prior to the expiration of the SDO permission period.

7.2 Programme

A programme can be found in Appendix B of this report.

8

Contractor Details

8.0 Contractor Details

8.1 Introduction

The purpose of this section of the report is to provide details of the appointed contractor carrying out the reinstatement works on behalf of the Site Operator, HMRC.

8.2 Contractor Details

The contractors to undertake the reinstatement work are yet to be appointed by HMRC. This will be subject to a competitive tendering exercise in accordance with The Public Contracts Regulations 2015 and OJEU (depending on the final financial threshold of the reinstatement works).

9

Site Monitoring

9.0 Site Monitoring

9.1 Introduction

The purpose of this section of the document is to provide details of any monitoring of the site and reinstatement work to ensure that the target condition of the site is achieved.

9.2 Site Monitoring

HMRC and their appointed project manager, Gleeds Building Surveying Ltd, will be undertaking regular site visits to ensure quality and compliance is maintained. The appointed contractor will also be requested to provide regular progress reports and attend site meetings.

10

Post Reinstatement Maintenance

10.0 Post Reinstatement Maintenance

10.1 Introduction

The purpose of this section of the document is to provide a report detailing how the site will be maintained post reinstatement, and by whom.

10.2 Post Reinstatement Maintenance

The site will be handed back to the Landlord, Birmingham Airport Limited, who will have ongoing responsibility for the site.

11

**Operational Management Plan &
Construction Management Plan**

11.0 Operational Management Plan and Construction Management Plan

11.1 Introduction

The purpose of this section of the plan is to describe where there are overlaps how the proposals align with commitments made in the Operational Management Plan (OMP) and Construction Management Plan (CMP) which were submitted as part of Article 4 of the SDO application.

11.2 Construction Management Plan

The approved Construction Management plan referenced below, dated 1st December 2020, was submitted as part of the Article 4 requirements of the SDO 2020. This plan includes the relevant methodologies for mitigating impacts on noise and air quality etc that could arise from the original construction of the Birmingham IBF site. The same mitigations apply to any reinstatement work. The approved Construction Management Plan is available for review upon request from HMRC.

The requirements of the Construction Management Plan relate to the SDO approved construction work to create the Inland Border Facility Site. The Reinstatement Work will be in accordance with this plan to ensure that those requirements, which were approved for the SDO, are maintained during the subsequent construction work required to reinstate. The SDO requirements of the Construction Management Plan (CMP) are outlined below:

No works of construction may commence until a construction management plan for the development has been submitted to and approved by the Secretary of State. The plan must comprise details of policies and procedures to be complied with in connection with the construction of the development in relation to:

- a) engagement with relevant owners and occupiers, including complaints handling.*
- b) application of best practicable means to minimise noise, vibration and emissions to air.*
- c) application of best practicable means to minimise the adverse effects of lighting on the amenity of relevant owners and occupiers, ecological receptors and road users.*
- d) sourcing, placing, managing and storing of construction materials, including, where appropriate, the stripping, storage and re-spreading of soil.*
- e) management of waste in accordance with the waste hierarchy.*
- f) pollution prevention and control*
- g) preventing damage to trees to be retained on the site, or trees immediately adjacent to any works on the site.*

h) management of construction traffic, including measures to prevent the deposit of mud and construction materials on the highway.

i) the management of invasive species. and

j) where appropriate, the appointment and retention of a suitably qualified archaeologist, arboriculturist, ecologist or ordnance specialist to oversee works

The work involved to reinstate the Birmingham IBF site, as described elsewhere in this plan, is relatively small scale so as a result those items detailed in the Construction Management Plan above will be minimal.

A response to each item:

- a) See Section 12 of this document.
- b) See Section 16 of this document.
- c) The lighting specific to the IBF will be removed, and the existing car park lighting will be reinstated to suit the required lux levels for its use. As the lighting being reinstated will be of a more modern fitting, it is likely that the location and quantity of fittings will be different.
- d) Few materials will be required for the reinstatement plan and where they are they will be delivered and stored on the site. Storage of these materials will be in accordance with the provision of the CMP:
 - Storage of any oil-based materials, including petrol, diesel, waste and vegetable and plant oil, and above ground fuel and oil storage tanks, the Principal Contractor will comply with the Control of Pollution (Oil Storage) (England) Regulations 2001, as amended, and the Environment Agency's PPG: Above ground oil storage tanks
 - Chemical storage, handling and use will comply with PPG 26: Drums and intermediate bulk containers
 - Stationary plant will be used with secondary containment measures such as plant nappies to retain any leakage of oil or fuel, which will be emptied at regular intervals to prevent overflow
 - As per CIRIA guidance, fuel will be stored in dedicated bunded, impervious storage areas at least 10m away from any water body including drains and watercourses. Please refer to Figure 4.1 (pg. 34) showing the distances from the fuel storage area in relation to existing drainage.
 - Fuel tanks will be stored within a bund capable of holding 110% of their capacity
 - Stockpiles and mounds will be kept away from sensitive receptors (including natural and historic features), watercourses and surface drains where reasonably practicable, and sites to take into account the predominant wind direction relative to sensitive receptors
 - Stockpiles and mounds will be maintained to avoid materials slippage
 - Materials stockpiles likely to generate dust will be enclosed or securely sheeted, kept watered or stabilised as appropriate

- Fine dry material will be stored inside buildings or enclosures with measures in place to ensure no escape of material and of overfilling during delivery
 - Maintain a clean and tidy site, ensuring all material is stored in the correct areas
- e) See Section 14 of this document.
- f) Due to the small-scale nature of the reinstatement work the levels of pollution will be far lower for this work than previously anticipated for the use of the IBF site.
- g) Not applicable, there are no trees within the immediate vicinity of the working areas on site.
- h) See Section 15 of this document.
- i) Due to the small-scale nature of the reinstatement work and disruption to natural habitats this is not anticipated to be a factor.
- Should any protected species be found during the works, works will stop immediately, and an ecologist contacted immediately via the Project Manager on-site.
 - Should invasive species such as Japanese Knotweed be encountered on-site, works will stop immediately, and the area fenced off. An ecologist will be notified via the Project Manager on-site for further action.
- j) See Sections 19 and 20 of this document.

11.3 Operational Management Plan

The requirements of the Operational Management Plan relate to the SDO approved operation of the Inland Border Facility Site. The Reinstatement Work will generally be in accordance with this plan to ensure that those requirements, which were approved for the SDO, are maintained during the subsequent construction work required to reinstate. The SDO requirements of the Operational Management Plan (OMP) are outlined below:

No use other than works of construction and tests of the operation of the development may commence until an operational management plan for the development has been submitted to and approved by the Secretary of State. The plan must comprise details of policies and procedures to be complied with in connection with the use and operation of the development...

- a. *Engagement with relevant owners and occupiers, including complaints handling*
- b. *Dealing with adverse weather, incidents and protestors*
- c. *Emergency response*
- d. *Inspection, maintenance and repair of hard surfacing, surface and foul water drainage systems, fire hydrants and emergency water supplies*

e. *Managing traffic associated with the operation of the development, including:*

- i. *the management of vehicles moving between the site and the SRN (being the highways for which Highways England is the highway authority, by virtue of article 2 of the Appointment of a Strategic Highways Company Order 2015(b)), and the provision of signage for their drivers and*
- ii. *preventing vehicles carrying goods specified in paragraph A.4 of Part 1 from having access to, or being stationed on, the site*

f. *Pollution prevention and control*

g. *Prescribing limits on levels of noise and emissions to air that will be adhered to during the operation of the development, and monitoring and management measures to secure adherence to those levels*

h. *Managing waste and waste disposal in accordance with the waste hierarchy*

i. *A local employment strategy for staff*

j. *Managing and enhancing biodiversity*

As per section 11.2, the work involved to reinstate the Birmingham IBF site, as described elsewhere in this plan, is small scale and so impact to the items outlined in the Operational Management Plan above will be minimal.

A response to each item:

- a) See Section 12 of this document.
- b) Not applicable.
- c) Not applicable.
- d) Not applicable.
- e) Not applicable but for construction traffic management see Section 15 of this document.
- f) Due to the small-scale nature of the reinstatement work the levels of pollution will be far lower for this work than previously anticipated for the use of the IBF site.
- g) See Section 16 of this document.
- h) See Section 14 of this document.
- i) Not applicable.
- j) Due to the small-scale nature of the reinstatement work and disruption to natural habitats this is not anticipated to be a factor.
 - a. Should any protected species be found during the works, works will stop immediately, and an ecologist contacted immediately via the Project Manager on-site and the appropriate action as advised by the ecologist will be taken.

- b. Should invasive species such as Japanese Knotweed be encountered on-site, works will stop immediately, and the area fenced off. An ecologist will be notified via the Project Manager on-site for further action and the appropriate action as advised by the ecologist will be taken.

12

Engagement with Relevant Owners and Occupiers

12.0 Engagement with Relevant Owners and Occupiers

12.1 Introduction

The purpose of this section of the document is to outline where community engagement for the proposals of the Reinstatement Plan has been necessary and undertaken. This section outlines who the relevant owners and occupiers are and how they could potentially be impacted by the planned reinstatement work.

Additionally, relevant statutory bodies (e.g. Environment Agency, Natural England) will be consulted where appropriate to do so.

Finally a complaints handling procedure is set out to detail how issues can be resolved if this isn't possible at a site level.

12.2 Engagement with Relevant Owners and Occupiers

The scale of the reinstatement work identified in this plan are minimal and time on site is to undertake this work has been estimated at 26 weeks, and those neighbouring stakeholders are likely to only experience minimal traffic for the work and large plant will only be required for the resurfacing works to bring the site back into use as a car park.

The owner and landlord of the site and surrounding area is Birmingham Airport Limited and they have been in regular communication with HMRC on the proposed reinstatement work and how it will impact them. There have been no other owners or occupiers identified in the immediate surrounding area.

Considering the extent of reinstatement work planned and it has not been deemed necessary to involve any statutory bodies.

The complaints handling procedure will be as agreed in the Article 4 submission of the SDO and as detailed below:

1. A sign will be placed in a prominent location close to the entrance of the IBF which will clearly provide details of how to make contact with the site and the contractors undertaking the reinstatement work on behalf of HMRC.
2. The email address for the site is 'complaints.birminghamibf@hmrc.gov.uk' and will be monitored while the site is occupied.

3. Complaints which are related to the reinstatement of the site will be received formally, either in writing, such as by email or letter, or verbally by phone or in person, and acknowledged upon receipt where possible (by email auto response, or an automated voicemail) or within a maximum of two business days (to manually issue a complaint reference number).
4. Details of all complaints received at the site and by whom will be noted by the member of staff who the complaint is made to, who will notify their line manager immediately, to ensure all required details have been captured.
5. Messages of complaint received at the site will always be recorded on an incident log. Verbal or in person complaints will be initially handled by the stakeholder's line manager who will endeavour to resolve the complaint at site level, at the time at which it is received. If a complaint cannot be easily resolved then the complaint will be logged into the Complaints Database and a Complaints number issued to the person making the complaint. The issuing of a complaints number is intended to be taken as formal acknowledgement of the complaint.
6. Complaints received in writing or by email will be addressed by HMRC and it is their responsibility upon receipt to ensure all complaints are recorded.
7. HMRC will be responsible for assessing the complaint as to its urgency, implications, and relevance to the site or to a Government Agency. If HMRC are unable to resolve the complaint at the time it is received, then they will advise the complainant of the eight business day timescale for a final response to be reached and provide them with a complaint reference number (if the complainant does not already have one)– which is intended to be taken as formal acknowledgement of the complaint.
8. Complaint responses will be handled by HMRC, who will triage, identify owners, and seek feedback from relevant stakeholder before sending a response to the complainant.
9. HMRC aim to have a final resolution to all complaints received within eight business days. Should a final response not be possible within the target period, such as an extended investigation or awaiting statements, the complainant will be contacted by HMRC within eight days to advise a revised timescale for resolution.
10. Each complaint will be handled fairly, confidentially and the issues will be investigated thoroughly before issuing the final response.
11. If a final response to a complaint been issued by HMRC but the complainant does not agree with the resolution determined by the outcome of the internal investigation, the complainant is able to raise an appeal. A senior leader from HMRC will review the complaint for a second time and respond within three working days. The complaint will again be handled fairly, confidentially and the issues will be investigated thoroughly before issuing another final response.
12. If the complainant remains unsatisfied with the resolution offered by the internal appeal process then the final avenue for escalation is to write to the local MP and ask for the matter to be referred to the Parliamentary Ombudsman. The complainant will be signposted to the local MP and the central database will capture actions taken.

13. It is possible that a complaint will be made verbally to any member of staff on-site (e.g. to a member of security staff at the entrance/exit to the site or by phone/email), therefore the following standards are required to be followed by staff when a complaint is being made to them:
- a. The recipient of the complaint will calmly listen to what is being said and remain objective. After hearing the complainant's full account of events, the recipient will reiterate the main points of the complaint back to the complainant to ensure all relevant details have been captured.
 - b. The complainant will not be challenged on the facts of what they are saying, listening and acknowledging what is being said is a key requirement, and the complainant must be thanked for getting in touch. The complainant must be asked what their expectations are in terms of what they feel would be an acceptable resolution, and this will be recorded by the recipient. However, to manage the complainant's expectations it must be made clear that while all effort will be made to resolve the complaint to the complainant's satisfaction, the resolution put forward by them may not necessarily be the one determined by the complaint investigation outcome.
 - c. Recipients are to take every complaint made seriously, they will record the complainant's details (name and preferred method of contact) and advise the complainant that the matter will be investigated in full.
 - d. The recipient will notify their line manager immediately that a complaint has been made to them and pass on all details they captured regarding the complaint.

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13

Site Safeguards

13.0 Site Safeguards

13.1 Introduction

The purpose of this section of the document is to detail any measures that will be used to protect any existing biodiversity on the site, and / or any biodiversity enhancement measures that have been agreed as part of the Biodiversity Enhancement Plan for the IBF (if any). This section will also outline the measures that will be put in place, if required, to ensure that during site reinstatement, biodiversity enhancements created alongside the provision of IBF sites will be protected.

13.2 Site Safeguards

As detailed in the original Article 4 submission, no significant effects are anticipated upon biodiversity from the decommissioning of the scheme.

14

Waste & Reuse of Materials

14.0 Waste & Reuse of Materials

14.1 Introduction

The purpose of this section of the document is to demonstrate the extent to which materials will be re-used and recycled off site. Where material is not being recycled details are provided for how they are being disposed of and how doing so meets the relevant industry standards such as the Waste and Resource Action Programme (WRAP).

14.2 Waste & Reuse of Materials

The top layer of the tarmac road surfacing that is proposed to be planed off will be reused. This material will be removed from site by the appointed contractor's specialist subcontractor. Road chippings and hard core have value and so will be taken away from site to be recycled/reused elsewhere.

All temporary accommodation, fixtures and fittings will be removed and used elsewhere. Where feasible HMRC are exploring using these items on other IBF sites, if this is not possible the units are generally leased/on hire and will be used by the supplier on other sites.

Skips and other storage receptacles used for the containment of construction, demolitions and excavation waste will be colour-coded in line with generic scheme developed by the Institution of Civil Engineers

Hazardous waste and non-hazardous waste must be stored separately. The mixing of hazardous and non-hazardous waste, either whilst stored on-site or upon collection will not be permitted

All skips and other storage receptacles will have appropriate signage to facilitate separation of waste for re-use, recycling, or disposal. Plastic sheeting will be used to prevent leaching from waste soils and aggregates where these are not contained within skips or other storage receptacles

All skips and storage receptacles will be sheeted, or otherwise remain lidded or closed, when waste is not being deposited into them. They will also be covered to prevent the escape of waste whilst in transit and loaded for maximum payload efficiency

All skips and storage receptacles will be inspected on arrival to ensure they are fit for purpose. Any not fit for purpose will be taken out of use immediately with appropriate signage to signify it will not be used

Liquid waste will be stored on hard-surfaced areas with secondary containment systems to prevent spillages

Waste will not be stored within 10m of any controlled watercourse, borehole, well, spring, surface water drainage system or foul water drainage system

15

Site Traffic and Considerate Construction Protocols

15.0 Site Traffic and Considerate Construction Protocols

15.1 Introduction

The purpose of this section of the report is to demonstrate how site traffic and other construction protocols will meet the requirements of the approved Construction Management Plan (CMP) advice note.

15.2 Site Traffic and Considerate Construction Protocols

Site traffic for reinstatement will be considerably reduced in comparison to the approved SDO use for HGV movement. Plant required on site will include those typically used for resurfacing works and HGVs to cart away waste from the disposal of road planings as part of the resurfacing work. Other anticipated vehicles include cranes and hiab vehicles to remove the temporary structures from site, and vans for operatives to travel to site in with their tools and materials for disconnecting services and making good surfaces.

16

Noise and Air Quality

16.0 Noise and Air Quality

16.1 Introduction

The purpose of this section of the report is to demonstrate that the relevant noise and air quality objectives, as detailed in the Construction Management Plan (CMP) and Operational Management Plan (OMP), are not breached during the reinstatement work.

16.2 Noise and Air Quality

Any noise and air pollution during the reinstatement of this site will be drastically reduced from what was approved for the use of the site as a temporary Inland Border Facility.

For the limited number of vehicles that will be required the following will be adhered to:

- The use of mains electrical power, hybrid generator systems and battery powered equipment where reasonably practical
- All plant on-site will be in good operating condition. Exhaust emission from all plant will be reduced as much as possible through effective and regular maintenance
- All construction vehicles will conform to the current EU emissions standards (NRMM Stage V compliance for all plant with an engine capacity of between 37-560kW and all generators above 560kW)
- All plant to be switched off when not in use
- No idling of vehicles
- Low sulphur diesel fuel will be used in all plant (non-road mobile machinery)
- The contractor will maintain a register of all plant on-site to record inspection and set up a system of regular inspection of all plant
- Vehicle and construction plant exhausts will be directed away from the ground and position them at a height to facilitate appropriate dispersal of exhaust emissions

17

Soil Handling for Site Reinstatement

17.0 Soil Handling for Site Reinstatement

17.1 Introduction

The purpose of this section of the report is to detail, where required, how conserved soils will be used for the restoration of land to its previous use, or subsequently agreed use. This includes setting out how soils will be handled, stored and used during site reinstatement.

17.2 Soil Handling for Site Reinstatement

Not applicable.

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18

Standards

18.0 Standards

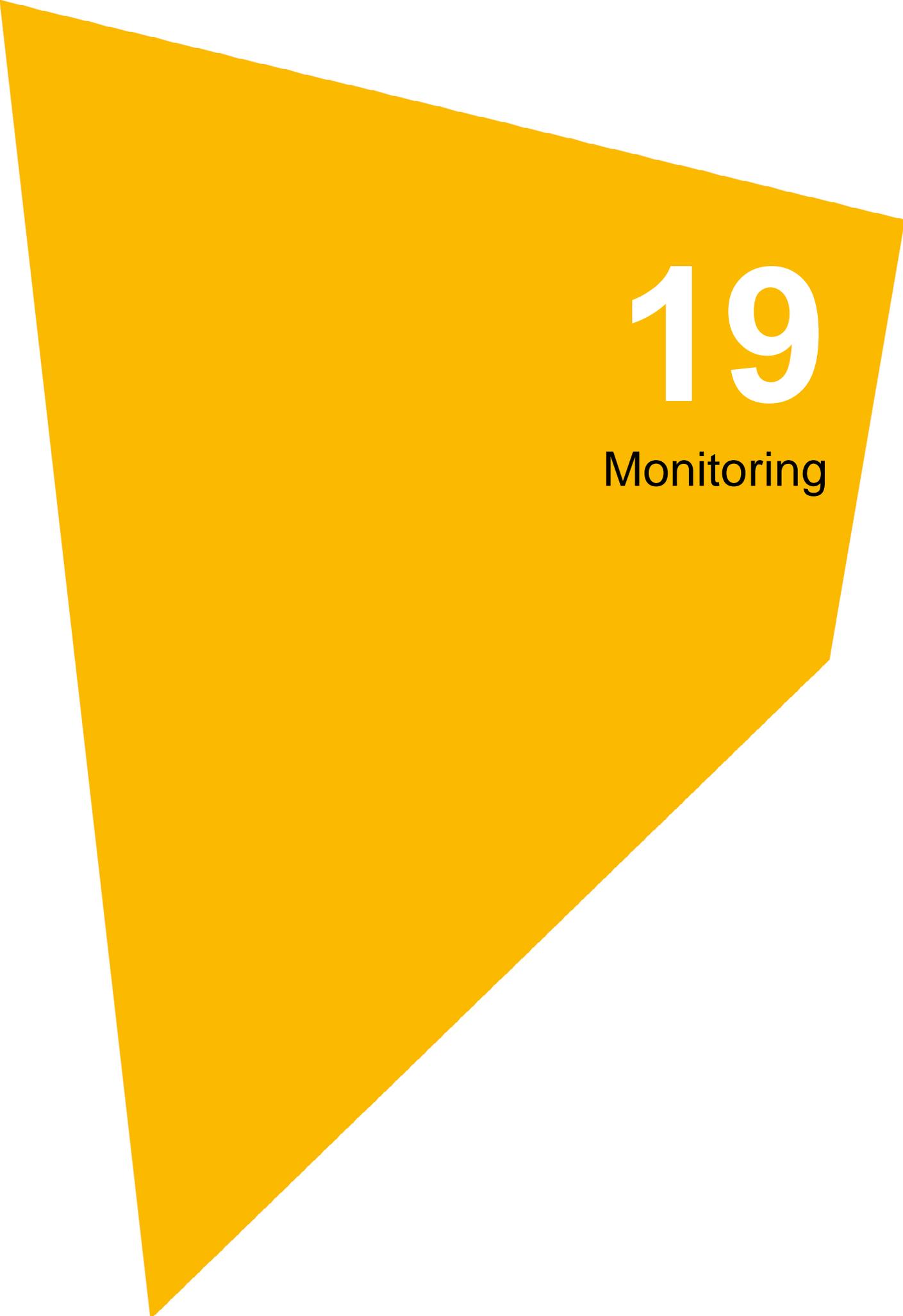
18.1 Introduction

This section of the report addressed the relevant British Standards that any reinstatement work should adhere to, in particular those related to planting and trees.

18.2 Standards

The following standards are mainly applicable to the resurfacing works required:

- BS EN 13108 Bituminous mixtures. Material specifications
- BS 594987 Asphalt for roads and other paved areas. Specification for transport, laying and compaction and type testing protocols
- BS EN 12697 Bituminous mixtures. Test methods for hot mix asphalt



19

Monitoring

19.0 Monitoring

19.1 Introduction

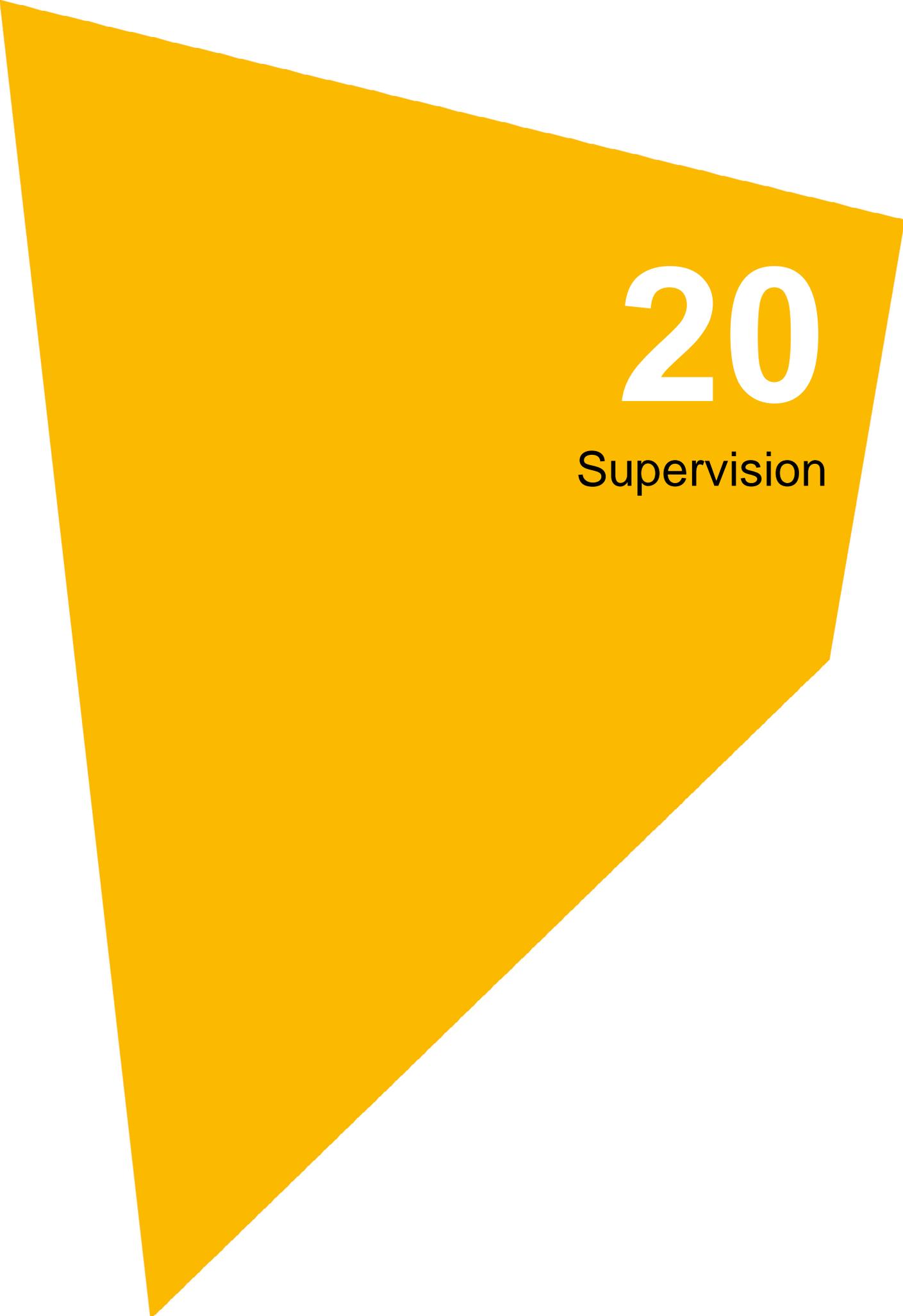
The purpose of this section of the document is to set out the proportionate measures and arrangements to monitor and manage the reinstatement work at this site. If relevant details are included detailing arrangements for additional work as may be required that are outside of the current scope to ensure the site achieve the target condition.

If required this section also provides detail for any post-handover monitoring, the party responsible, the extent of any liabilities and timescale for ongoing monitoring.

19.2 Monitoring

Gleeds Building Surveying Ltd have been appointed to project manage the reinstatement of this site. The process will be monitored by them and HMRC to ensure that timely progress is made by the appointed contractor so that they meet the programme.

Time has been allowed in the programme for any unforeseen work that may be required on this site to reinstate it.



20

Supervision

20.0 Supervision

20.1 Introduction

The purpose of this section of the document is to provide details of the suitably qualified and experienced personnel who will be responsible for supervising the reinstatement work, outside of the principal contractor role.

20.2 Supervision

Gleeds Building Surveying Ltd have been appointed to project manage this process and senior chartered surveyors will be responsible for supervising the reinstatement of this site.



21

Handover

21.0 Handover

21.1 Introduction

The purpose of this section of the document is to provide details of who the reinstated site will be transferred to upon completion of the reinstatement works. If required, this section will also detail the period of time for which the contractor is responsible for replacing any landscape planting (e.g. trees and shrubs) which may fail after initial planting.

21.2 Handover

The site will be handed back to the Landlord, Birmingham Airport Limited, upon completion of the reinstatement work and termination of the lease. Birmingham Airport Limited will be responsible for ongoing maintenance of the site.

There is no intended planting work as part of this reinstatement, other than the proposal for offsite planting to substitute the proposed replanting of the low quality trees previously planted between car parking spaces.

22

Other Site Specific Requirements as
Detailed in Advice Note

22.0 Other Site Specific Requirements as Detailed in Advice Note

22.1 Introduction

This section of the document outlines the measures detailed in the REAC which are required to be addressed as part of the Reinstatement Plan. Full details can be found within Appendix C.

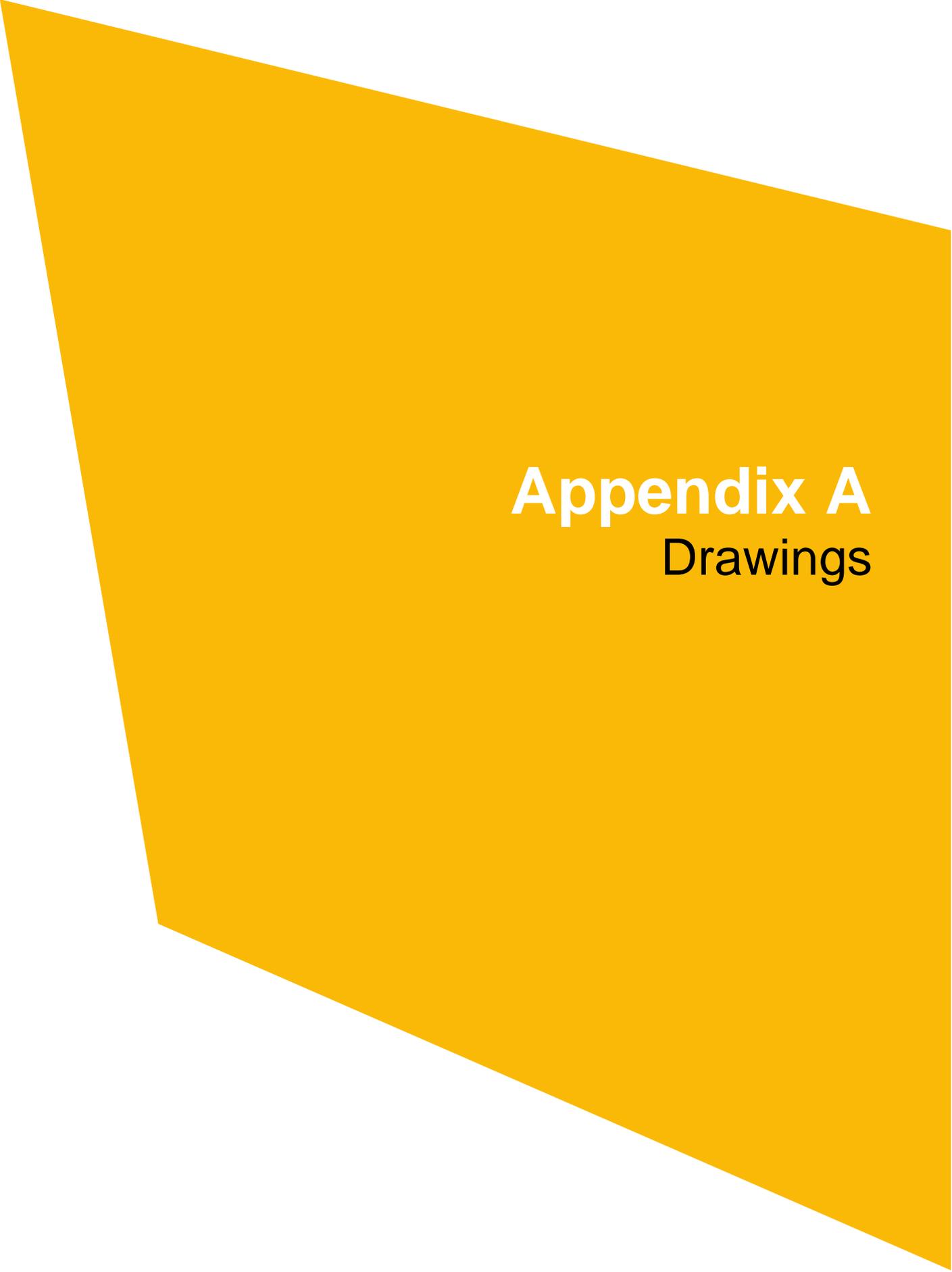
22.2 Other Site Specific Requirements

The requirements from the REAC for the items relevant to the Reinstatement Plan are as follows:

Condition	Response
AQ1	<p>It is not anticipated that the removal of the temporary structures will create significant dust emissions during reinstatement, however dust is likely to be created from the resurfacing works to bring the site back into use as a car park. The following best practice measures will be adopted by the contractor:</p> <ul style="list-style-type: none"> • Minimising stockpile heights and profiles to limit wind-blown dust emissions • Vehicle with open loads to be securely sheeted or enclosed • Maximum speed limit on site to prevent the generation of dust by fast moving vehicles • Damp down surfaces in dry conditions • Vehicle engines and plant motors to be switched off when not in use
L1	During reinstatement the contractor is to ensure that the site is well managed and tidy at all times, governed by an appropriate construction phase plan for the reinstatement works.
L2	No further vegetation removal is anticipated as part of the reinstatement works
L4	The vegetation removed to the perimeter of the site to facilitate security of the site and to allow services installation is to be reinstated on a like for like basis supported by an appropriate planting specified from a suitably qualified landscape architect. The existing low quality trees between car parking bays are to be planted off site via an agreement with SMBC to support their Local Plan.
GS1	Best practice measures will be adopted by the contractor during reinstatement including the provision of spill kits with reinstatement staff trained in their correct application.
GS2	Risk assessments specific to the reinstatement works will be provided in order to identify risks and appropriate mitigation measures in line with all the relevant health and safety legislation and guidance, to ensure the safety of workers.

B1	The vegetation removed to the perimeter of the site to facilitate security of the site and to allow services installation is to be reinstated on a like for like basis supported by an appropriate planting specified from a suitably qualified landscape architect. The existing low quality trees between car parking bays are to be planted off site via an agreement with SMBC to support their Local Plan.
B2	No vegetation removal is proposed as part of the reinstatement works so this is not anticipated to have a negative impact on nesting birds or any other species or habitats. Any excavations carried out as part of the reinstatement work that are left overnight will be covered or ramps installed so that mammals do not become trapped.
B4	No vegetation removal is proposed as part of the reinstatement works so this is not anticipated to have a negative impact on existing species and habitats. Dust suppression methods will be undertaken in line with AQ1 to avoid dispersal and minimise any effect on the surrounding habitat.
M1	The reinstatement contractor is to ensure that the principals of the waste hierarchy is followed when dealing with was on site including prevention, reuse and preparation for reusing, recycling, recover and disposal.
NV1	The reinstatement contractor is to ensure all noisy operations required as part of the reinstatement would be completed between 0700 and 1900 on weekdays and 0800 to 1700 on Saturdays. All noise emitting equipment will be switched off when not in use.
NV2	The following noise mitigation measures will be implemented as part of the reinstatement works: <ul style="list-style-type: none"> • Equipment to be maintained and in good working order • Fitting equipment with silencers or muddlers where appropriate • Management of deliveries to prevent unnecessary queuing traffic into site • Lowering of materials instead of being dropped from a height • Use of adjustable or directional audible vehicle reversing alarms
NV3	HMRC are to ensure that the local community are informed of the reinstatement proposals through their existing communication channels such as providing details on a website, site notices and letter drops where appropriate.
PH1	HMRC are to ensure that the local community are informed of the reinstatement proposals through their existing communication channels such as providing details on a website, site notices and letter drops where appropriate. HGV movements are to be controlled on site by appropriate site marshals.
RDWE1	As part of the reinstatement works the contractor will manage the works in accordance with the CIRIA guidelines on best practice in relation to water management such as: <ul style="list-style-type: none"> • Reinstatement workers to be briefed on the use of spill kits as part of their site induction • All plant and machinery to be maintained in a good condition • Pollution prevention and spill response procedures to be developed by the contractor • Dust suppression in accordance with AQ1 of the REAC as above

	<ul style="list-style-type: none"> Retention of penstock valve to further mitigate potential adverse effects during reinstatement works and once the site is handed back to the landlord
C1	<p>The contractor is to reduce carbon emissions as part of the reinstatement works by including the following within their working practices:</p> <ul style="list-style-type: none"> Transportation of materials to site using low-carbon modes where possible Use of low carbon construction materials required for the reinstatement Provision to effectively segregate waste during the reinstatement works
C3	<p>As part of the reinstatement opportunities will be explored for the temporary structures (offices and inspection sheds) to be sold and re-used elsewhere where they are not leased or rented.</p>

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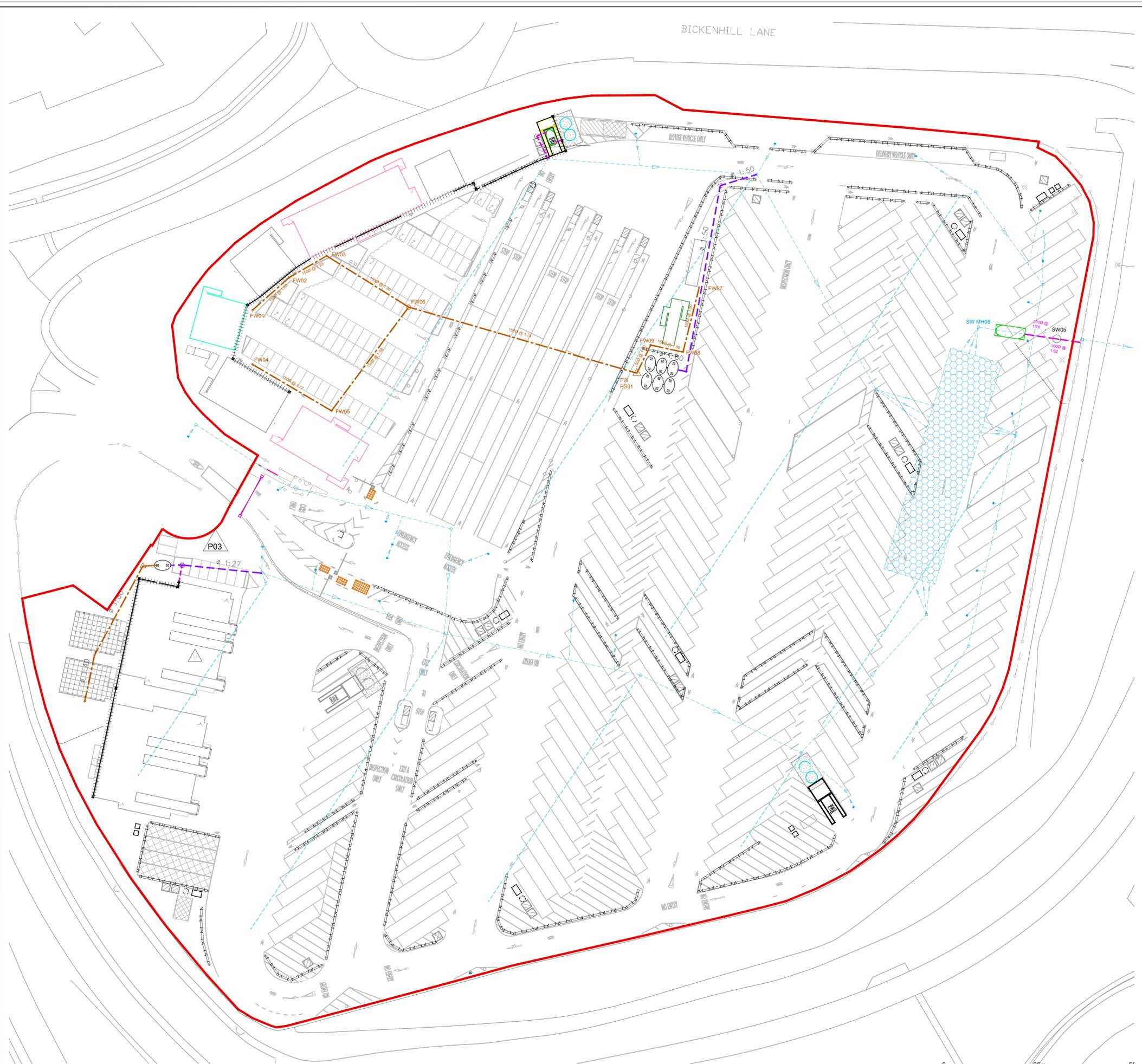
Appendix A

Drawings

BICKENHILL LANE

Key to symbols

	Existing surface water drainage and manhole
	Existing linear drainage
	Existing road gully
	Existing attenuation tank (location shown approximately)
	Foul water drainage and manhole
	Foul water pumping station
	Foul water rising main
	Surface water drainage and manhole
	Treated effluent and manhole
	Linear drainage channels & access point (designed by others)
	Oil separator



Rev.	Description	By	Date



Project
**Birmingham IBF
 Reinstatement Plan**

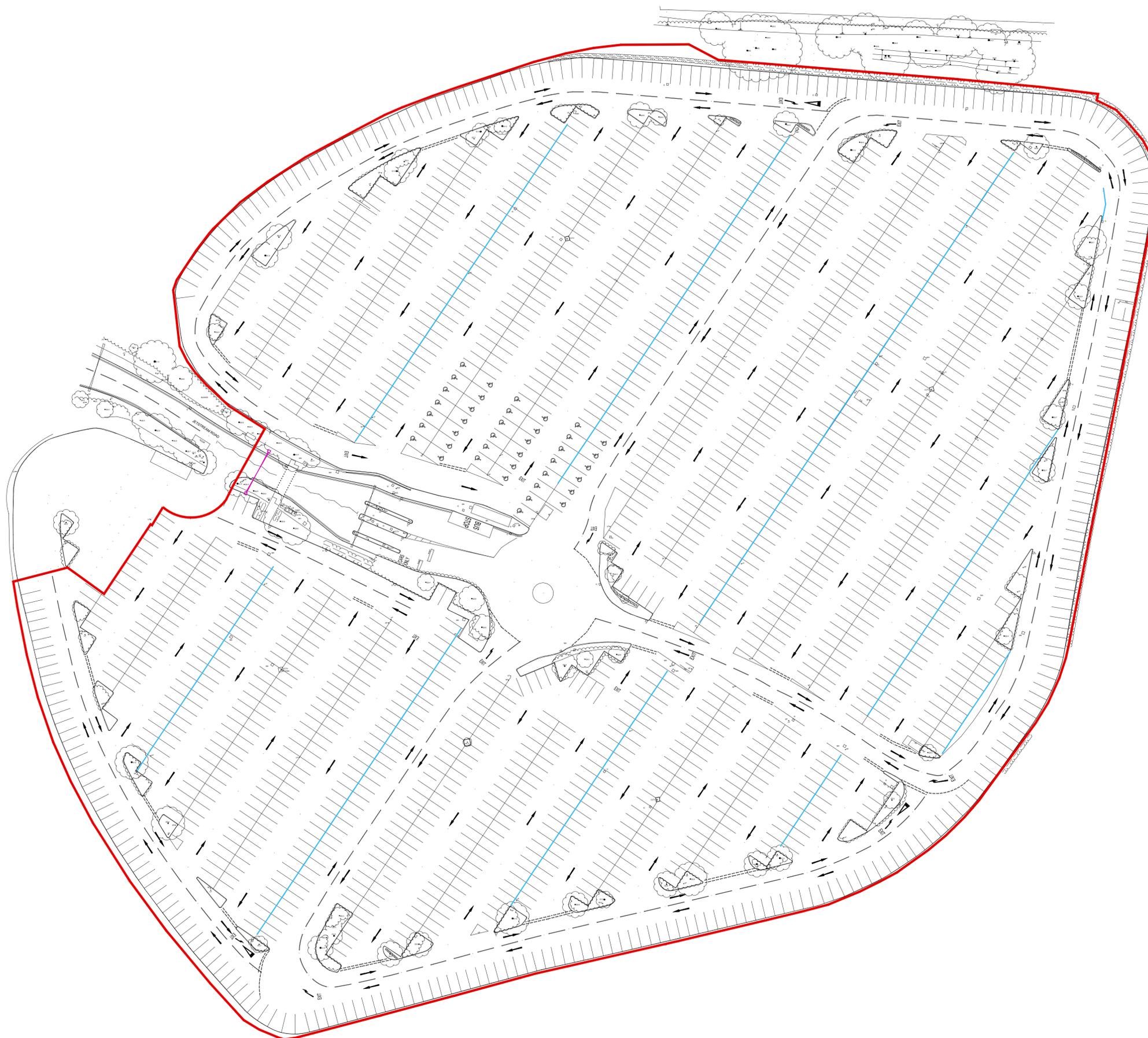
Drawing Title
Existing Plan

Drawing No. **NTBS3315/01**

Scale **1:500** Original Sheet Size A1

Drawing Status

gleeds	Drawn	Checked
	SA	TS
Date	Rev.	
03/2022		



Rev.	Description	By	Date
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Client



Project

Birmingham IBF
Reinstatement Plan

Drawing Title

Proposed Plan

Drawing No.

NTBS3315/02

Scale

1:500

Original Sheet Size A1

Drawing Status

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Drawn	Checked
SA	TS
Date	Rev.
03/2022	



Class 1 by-pass separator retained
 Pollution control chamber retained
 Existing oil separator

Rev.	Description	By	Date
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Client



Project

Birmingham IBF
 Reinstatement Plan

Drawing Title

Retained Below Ground Drainage

Drawing No. NTBS3315/03

Scale 1:500 Original Sheet Size A1

Drawing Status

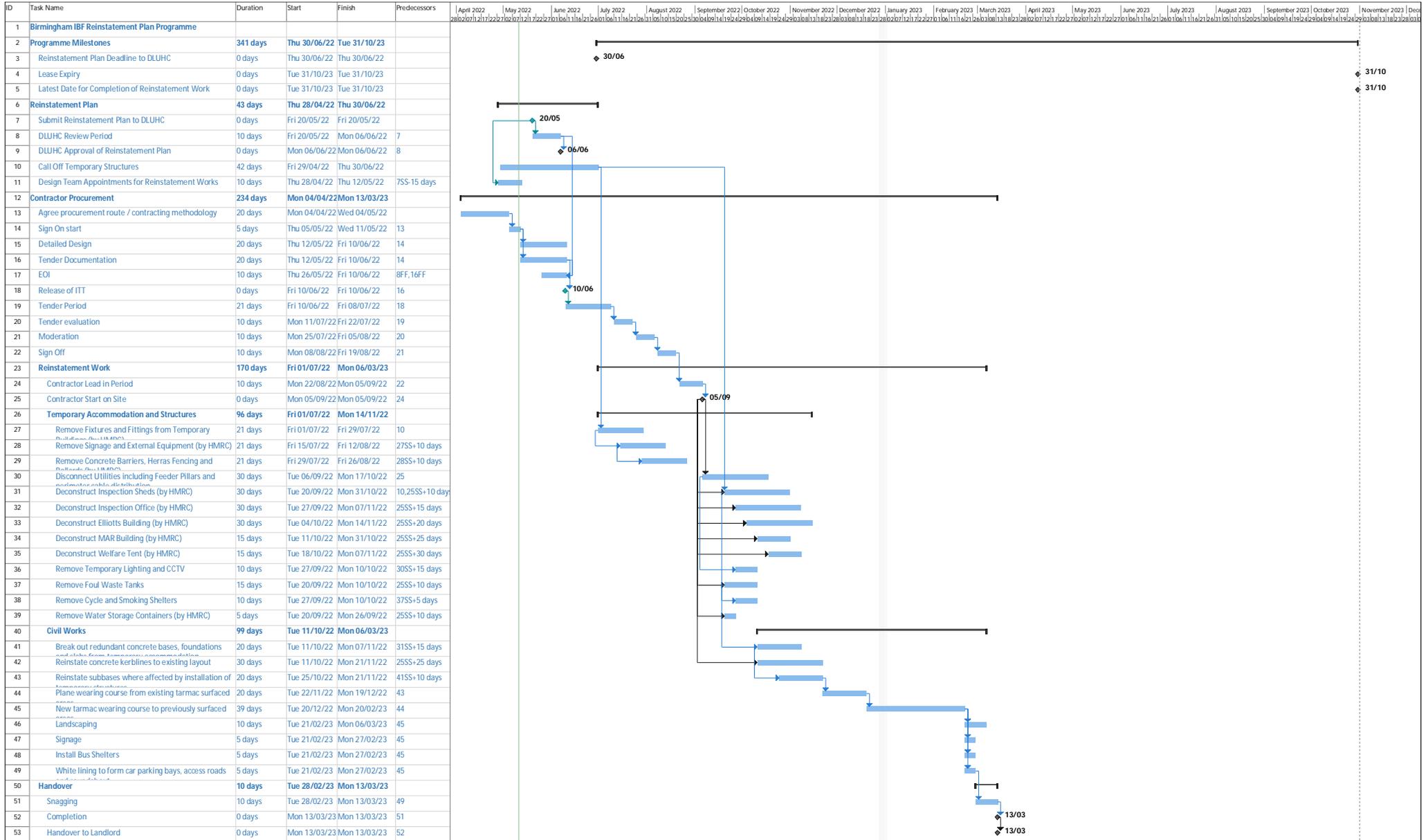
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Appendix B

Programme



Project: 220330 Birmingham IBF

Task Split

Milestone Summary

Project Summary Inactive Task

Inactive Milestone Inactive Summary

Manual Task Duration-only

Manual Summary Rollup Manual Summary

Start-only Finish-only

External Tasks External Milestone

Deadline Progress

Manual Progress

Appendix C

Record of Environmental Actions and Commitments (REAC)

B. Record of Environmental Actions and Commitments

B.1 Purpose of the Record of Environmental Actions and Commitments

This Record of Environmental Actions and Commitments (REAC) has been produced to support the environmental report for the scheme.

The REAC contained in 6 identifies the environmental commitments included within the AALEEDR that are required to address the potential environmental effects of the scheme. This is the main vehicle for passing essential environmental information to the Client and crucially to the body responsible for construction, future maintenance and operation of the asset.

Table B.1 – Record of Environmental Actions and Commitments

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
Air Quality (AQ)					
AQ1	To limit and control dust emissions during construction	<p>Works would be carried out in accordance with Best Practicable Means, as described in Section 79 (9) of the Environmental Protection Act 1990, to reduce the creation of dust on-site during the construction phase. This would include:</p> <ul style="list-style-type: none"> • Minimise height of stockpiles and profile to minimise wind-blown dust emissions and risk of pile collapse. • Locate stockpiles out of the wind (or cover, seed or fence) to minimise the potential for dust generation. • Ensure that all vehicles with open loads of potential dusty materials are securely sheeted or enclosed. • Enforce a maximum speed limit of 15mph on surfaced roads and a 10mph speed limit on unsurfaced haul roads and work areas, to prevent the generation of 	Not applicable	Daily site audits	Principal Contractor

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<p>dust by fast moving vehicles.</p> <ul style="list-style-type: none"> • Damp down surfaces in dry conditions. • All vehicle engines and plant motors shall be switched off when not in use. 			
Landscape (L)					
L1	To limit visual intrusion and impacts upon landscape character during construction	<p>The following measures to be undertaken to reduce visual intrusion and impacts upon the landscape throughout construction:</p> <ul style="list-style-type: none"> • Keep a well-managed and tidy site. • Welfare units and temporary site offices in a colour that would aid integration with the surrounding landscape where possible. 	Not applicable	Daily site audits	Principal Contractor
L2	To ensure no significant effect during vegetation removal	<p>Prior to vegetation removal an arboricultural method statement is to be produced by an appropriately qualified arboriculturist, which would identify how retained trees would be protected during the works.</p>	Vegetation clearance is required	Condition of the SDO	Operator

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
L3	To limit impacts on landscape character during operation	Any vegetation that is required to be removed to facilitate the scheme shall be replanted on expiry of the SDO (at the end of the operational phase). This planting should be replaced on a like-for-like basis supported by an appropriate planting specification.	Vegetation clearance is required	Condition of the SDO	Operator
Geology and Soils (GS)					
GS1	The management of soil and groundwater contamination risks	<ul style="list-style-type: none"> Should any hazardous materials be encountered during construction, all materials would be dealt with. Hazards arising from hazardous cargoes during operation would be mitigated by segregating vehicles with hazardous loads as far as possible from all other vehicles. Any fuels, oils or hazardous materials used during the works would be appropriately stored and kept in bunded areas to prevent pollution of surface and ground waters. Spill kits shall be provided on-site for the duration of the works and construction staff trained in their correct application. 	Not applicable	Daily site audits	Principal Contractor / Operator

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
GS2	Management of contamination risks: workers	Production of risk assessments specific to the works in order to identify risks and appropriate mitigation measures in line with all the relevant health and safety legislation and guidance, to ensure the safety of workers.	Construction activities pose a risk to workers on-site	Production of and adherence to risk assessments	Principal Contractor
Biodiversity					
B1	To ensure no net biodiversity loss	Any vegetation that is required to be removed to facilitate the scheme shall be replanted on expiry of the SDO.	Assume that vegetation clearance is required	Condition of the SDO	Operator
B2	To ensure no effects to protected species	<ul style="list-style-type: none"> All vegetation removal should be supervised by a suitably qualified ecologist and be undertaken outside of the main breeding bird season (and therefore, should be undertaken between September to February). In the event that any excavations are required, ensure that these are covered over night or that ramps are installed so that mammals do not become trapped. All work should aim to retain as much vegetation as possible. 	Protected species may be present	Ecological Clerk of Works on-site	Ecological Clerk of Works

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<ul style="list-style-type: none"> Vegetation removal should be undertaken outside of the main breeding season (undertake clearance between September and February) where possible. If vegetation removal/disturbance is required between March and August inclusive, a suitably qualified ecologist will be required undertake a nesting bird check prior to works commencing. Works are expected to remove the trees / vegetation on site. 			
B3	To ensure no significant effect on bats	<p>The following measures would be required during construction and operation:</p> <ul style="list-style-type: none"> To use low or high-pressure sodium lamps, instead of mercury or metal halide lamps, with the use of glass glazing preferred where possible. The use of Light Emitting Diodes (LEDs) should also be used in preference to mercury or metal halide lamps; It is recommended that a Preliminary Roost Assessment (PRA) is 	Bats are using the vegetation along the east of the site for foraging and commuting	Technical note on the bat surveys to inform the lighting strategy	Consultant

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<p>undertaken on trees to be removed.</p> <ul style="list-style-type: none"> • To fit lighting at as low a height as is practicable; • In the case where lighting is required for security reasons, the use of lights with motion detectors or the use of Intelligent Video Analytics (IVA), which uses infra-red to detect movement, should also be considered. This would work to ensure areas are only lit when necessary; and, • To minimise the upward spill of lights with the use of directional luminaries, shields, louvres and baffles. This would direct light to where it is required and prevent unnecessary light spill into the surrounding environment. • A dark corridor is maintained along the vegetated bund which runs along the eastern boundary of the site. This will help maintain the existing bat foraging and commuting habitat. 			

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
B4	To provide net gain in biodiversity	<p>To meet the obligations of the NERC Act 2006 and the NPPF 2019:</p> <ul style="list-style-type: none"> • An effort should be made to retain as much habitat as possible in the area surrounding the works; • Any retained habitat should be protected during construction; tree root protection areas (RPAs) for any trees retained should be maintained. • Dust suppression measures should be applied to avoid dispersal and minimise any effect on the surrounding habitat. • Any habitats cleared outside the final footprint of the works should be reinstated using native seed mixes or native nectar producing plants appropriate to the surroundings. • It is not recommended that bat and bird boxes be installed on retained trees on-site. 	Not applicable	Not applicable	Operator
Material Assets and Waste (M)					

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
M1	Ensure appropriate waste management facilities are identified and used	Where material must be taken to a recycling or disposal site, these sites must have the appropriate permits and should be located as close to the works as possible.	Assumes waste infrastructure has capacity locally	Not applicable	Detailed design consultant
M2	Ensure appropriate waste management during operation	Ensure waste bins are appropriately placed throughout the operational area.	Assumes waste skips would be provided	Waste skips are used by Heavy Goods Vehicles (HGVs)	Operator
M3	Ensure principals of the waste hierarchy are adhered to	Where possible, ensure that the waste hierarchy is followed when dealing with waste on-site: prevention, reuse and preparation for reuse, recycle, recovery, and disposal.	Assumes appropriate waste bins are available for waste separation	Waste audits	Operator
Noise and Vibration (NV)					
NV1	Hours of working during construction	All noisy operations would be completed between 0700 and 1900 on weekdays, and 0800 to 1700 hours on Saturdays, occasionally on Sundays, switching off noise-emitting equipment when not in use and the use of temporary noise barriers where appropriate. Where out of hours working is required, prior agreement would be sought with Solihull Borough Council.	Not applicable	Daily site audits	Principal Contractor
NV2	Limit noise emissions during construction	Implement the following noise mitigation measures during construction:	Not applicable	Daily site audits	Principal Contractor

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<ul style="list-style-type: none"> ▶ Ensure equipment is maintained, in good working order, and is used in accordance with the manufacturer's instructions. ▶ Fit equipment with silencers or mufflers. ▶ Manage deliveries to prevent queuing of site traffic. ▶ Do not leave plant running unnecessarily. ▶ Careful orientation of plant with directional features. ▶ Materials to be lowered instead of dropped from height. ▶ Use of adjustable or directional audible vehicle-reversing alarms or use of alternative warning systems (for example, white noise alarms). ▶ Train and advise members of the construction team during toolbox talk briefings on quiet working methods. ▶ Erect temporary barriers to fully obscure the construction works from nearby receptors. 			

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
NV3	Mitigate effects of noise and vibration on local communities	Ensure the local community are informed of the proposals.	Not applicable	Not applicable	Principal Contractor
NV4	Limit noise emissions during operation	Ensure that vehicle idling does not occur during operation.	Not applicable	Not applicable	Operator
NV5	Limit noise emissions during operation	Minimise period that generators are used onsite once operation commences. As soon as connection is available, all electricity should be supplied through a dedicated power supply.	Not applicable	Not applicable	Operator
Population and Health (PH)					
PH1	Reduce effects on local community	<p>The following measures would reduce effects on the local community during operation:</p> <ul style="list-style-type: none"> • Ensure that HGV movements are controlled on-site by marshalling. • Ensure local community informed of the proposals. 	Not applicable	Not applicable	Operator
Drainage and the Water Environment (RDWE)					
RDWE1	To mitigate potential adverse effects upon RDWE during construction	<p>Construction activities will be managed in accordance with CIRIA Guidelines. Guidance on best practice in relation to pollution prevention and water management is set out in the following documents:</p> <ul style="list-style-type: none"> • CIRIA's Environmental good practice on-site. 	Not applicable	Daily site audits	Principal Contractor

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<ul style="list-style-type: none"> • Environment Agency's Protect groundwater and prevent groundwater pollution. <p>Measures to be implemented to limit the impact of construction activities on the water environment include:</p> <ul style="list-style-type: none"> • All construction workers to be briefed on the use of spill kits as part of the site induction. • Any stockpiled materials to be stored within enclosed areas to enable the runoff to be stored and treated where required. • All plant and machinery to be maintained in a good condition and any maintenance required would be undertaken within safe areas. • Pollution prevention and spill response procedures to be developed by the contractor and a spill kit and clean up equipment maintained on-site. • Dust suppression measures as described in AQ1 of this REAC. 			

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
RDWE2	To mitigate potential adverse effects upon RDWE during operation	PSTP will be installed on site. Treated effluent will be tankered off site and disposed of in accordance with authority requirements while a Consent to Discharge Permit is applied for with the EA. When a permit is granted, the treated effluent will then discharge into the surface water drainage system on site and via the outfall into receiving watercourse under EA permit conditions.	Not applicable	Not applicable	Principal Contractor / Operator
Climate (C)					
C1	To reduce carbon emissions associated with the scheme during Construction, Decommissioning and reinstatement	<p>The carbon reduction principles as detailed within Section 3 of the Carbon Assessment and Reduction Report, Appendix I, would be considered including the following:</p> <ul style="list-style-type: none"> • Transportation of materials to site would prioritise low-carbon modes where possible • Where possible, low-carbon construction materials and products will be preferred • Where possible low-carbon construction plant and equipment would be used 	Not applicable	Not applicable	Principal Contractor and Reinstatement Contractor

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<ul style="list-style-type: none"> Provision would be made to enable waste to be effectively segregated during construction, enabling materials to be effectively managed using the waste hierarchy, prioritising re-used and recycling over disposal. <p>Circular economy principles, such as Modern Methods of Construction, would be implemented, where possible.</p>			
C2	To reduce carbon emissions associated with the scheme during Operation	<p>The carbon reduction principles as detailed within Section 3 of the Carbon Assessment and Reduction Report, Appendix I, would be considered including the following:</p> <ul style="list-style-type: none"> Provision would be made to enable waste to be effectively segregated during operation, enabling materials to be effectively managed using the waste hierarchy, prioritising re-used and recycled over disposal. Where possible, measures would be put in place to limit profligate energy use by unintended user behaviours e.g. using 	Not applicable	Not applicable	Principal Operator

Reference	Objective	Action (including specific location and any monitoring required)	Assumptions (on which the action is based)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
		<p>motion sensors to control lights</p> <p>Where possible, measures would be put in place to limit profligate water use by unintended user behaviours e.g. using aerated taps.</p>			
C3	To reduce carbon emissions associated with the scheme during Decommissioning and Reinstatement	<p>Resource efficiency would be maximised through decommission and reinstatement.</p> <p>Opportunities for the reuse of assets following the end of operation would be explored as a priority. If reuse is not possible then recycling would be maximised.</p>	Not applicable	Not applicable	Principal Operator and Reinstatement Contractor

