

Ebbfleet Inland Border Facility Reinstatement Plan

Town and Country Planning (Border Facilities and Infrastructure)
(EU Exit) (England) Special Development Order 2020

His Majesty's Revenue and Customs
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1

Introduction

1.0 Introduction

Have been appointed by His Majesty's Revenue and Customs (HMRC) to produce a Reinstatement Plan for the Ebbsfleet Inland Border Facility (IBF). The Reinstatement Plan is a requirement of the Town and Country Planning (Border Facilities and Infrastructure) (EU Exit) (England) Special Development Orders of 2019 and 2020 (SDO) and this must be approved by the Secretary of State for the Department for Levelling Up, Housing and Communities (formally the Ministry of Housing, Communities and Local Government (MHCLG)) prior to any reinstatement work on this site. The definition of 'reinstatement works' in the SDO means 'the cessation of the use, and removal of all buildings, for which planning permission is granted by article 3(1)(a) and (b) on a site and the restoration of the site to its condition before such development commenced, except as specified in a reinstatement plan approved by the Secretary of State'.

This report has been produced for the benefit of The Department for Levelling Up, Housing and Communities (DLUHC) to determine the state of the Ebbsfleet Inland Border Facility (IBF) following cessation of IBF operation, reinstatement, and handover.

Approval was granted via a Special Development order on 20 November 2020 by the then MHCLG and is known as Town and Country Planning (Border Facilities and Infrastructure) (EU Exit) Special Development Order 2020 which includes the site known as International Station Car Park D, International Day, Swanscombe, DA10 1EB. The approval provided consent for a temporary development comprising the temporary change of use of the site from an existing car park to a temporary Inland Border Facility to provide 256 HGV spaces and 142 staff parking spaces for functions required to be carried out by border departments.

This report relates to the area occupied by HMRC and as shown on the red line plan contained within Appendix A. This Reinstatement Plan defines the planned reinstatement work at Ebbsfleet IBF with commentary and drawings identifying where and what work is required to reinstate the site to its former condition prior to the 2020 SDO and where HMRC intend to retain site features and the justification for doing so.

A general summary of those items to be removed/reinstated as detailed within the original Article 4 Decision Notice are:

- Removal of Temporary modular office buildings, two inspection sheds and marshals cabins
- Removal of 10m and 12m tall lighting columns, heras and palisade security fencing and CCTV
- Removal of dedicated access/egress, as a vehicular cross-over of the footway, to the British Transport Police (BTP) compound and conversion of existing, one-way spine road to two-way operation from the car park entry to the BTP compound
- Reinstatement of a small quantity of vegetation around the site

- Reinstatement of kerbs and ornamental trees throughout the existing car park previously removed to accommodate circulation routes and lorry parking areas
- Reinstatement of minor alternations made to the junction of International Way and existing site egress

A general summary of those items to be removed/reinstated as consented under the SDO 2019:

- Removal of a single 10 bay modular office unit for Border Force use
- Removal of a single 4 bay modular officer unit for Marshals and Security Staff
- Removal of connections of power, water and wastewater utilities, including power supplied by generator, water via above ground water tanks and waste to above ground septic tanks.
- Removal of gate booths at the entrance and exit
- Removal of ANPR cameras at the entrance and exit
- Other minor works

A general summary of those items to be retained are:

- Western boundary access from International Way for HGV access onto site
- Automated penstock valves to the surface water drainage system

2

Description of Reinstatement Work

2.0 Description of Reinstatement Work

2.1 Introduction

The purpose of this section of the report is to provide a clear description of the site prior to occupation, the proposed reinstatement work and the target condition of the site following the reinstatement.

2.2 Site Description/History Prior to Occupation

Prior to occupation, the site was known as Ebbsfleet International Car Park D and was originally constructed for use as a car park to service the adjacent international train station.

The site extends to approximately 8.7 hectares and is bounded by International Way to the North and West, with disused scrubland to the Eastern and Southern boundaries. The site previously comprised of predominantly tarmac hardstanding with raised hardstanding and vegetation islands to form car parking rows. The site is bound by a paladin weldmesh fence with vegetation including trees set immediately outside the majority of the boundary line. The British Transport Police (BTP) occupy the North East corner of the site comprising of modular accommodation and car parking, and did not form part of the Article 4 submission or this Reinstatement Plan.



Site Location Map

The Site was originally granted consent for development in 1996, where the Channel Tunnel Rail Link (CTRL) Act 1996 granted approval for Ebbsfleet International Station and the associated car parking (including Car Park D). Section 9 of the Act permitted the provision of 9,000 spaces. An application for a reserved matter under the CTRL Act 1996 was submitted for the siting of the Ebbsfleet Station car parks (in June 2001), with further applications submitted for the layout, design, external appearance and landscaping. Temporary uses on the Site, for activities other than the use of a car park have been approved previously, with the approval for use of the Site as a vehicle stunt show in 2015.

The prior use of the site as a car park was reliant upon the following planning permissions:

Planning Application Reference	Location	Description of Development	Date Permission Granted
20010495	Land At Ebbsfleet Northfleet Kent (Gravesham Package 23: Reserved Matters)	Application for the approval of the siting of Ebbsfleet Station and car parks within the Borough of Gravesham	Approved January 2002
Channel Tunnel Rail Link Act 1996		An Act to provide for the construction, maintenance and operation of a railway between St. Pancras, in London, and the Channel Tunnel portal at Castle Hill, Folkestone, in Kent, together with associated works, and of works which can be carried out in conjunction therewith; to	Enacted 18 December 1996

The site will revert back to its previous planning consent under planning reference 20010495 as use as a car park for the adjacent Ebbsfleet Station.

2.3 Description of Reinstatement Work

The reinstatement work is intended to revert the site back to its state prior to HMRC occupation with some beneficial features being retained which are described and justified in Section 3 of this document.

2.3.1 Temporary Accommodation

The temporary accommodation installed to enable the site to operate is all to be removed and surfaces disturbed as a result of installation will be made good. These units are listed below:

Item	Reason for removal
HMRC Offices	Temporary Accommodation
Marshals Facilities	Temporary Accommodation
Examination Facilities	Temporary Accommodation
Examination Offices	Temporary Accommodation
Drivers Office	Temporary Accommodation
Control Building	Temporary Accommodation
Drivers Facilities	Temporary Accommodation
Security Cabins	Temporary Accommodation
Shipping Containers	Temporary Accommodation
Smoking Shelters	Temporary Accommodation
Cycle Shelters	Temporary Accommodation
Raised Gate Booths	Temporary Accommodation
Generators & Assoc. Equipment	Installed for IBF site operation, no longer required
Access Ramps & Stairs	Installed for IBF site operation, no longer required
Water and Waste Tanks	Installed for IBF site operation, no longer required
Concrete Barriers	Installed for IBF site operation, no longer required

2.3.2 Services

Site facilities and temporary accommodation are predominantly serviced by localised on site generators with associated feeder pillars and internal distribution boards, all of which will be removed as part of the reinstatement works.

New lighting columns that were installed in accordance with the SDO will be removed. All existing lighting columns that previously serviced the car park were left in situ and will remain as part of the reinstatement works.

The CCTV network including columns and equipment will be removed from site and disturbed areas reinstated.

As foul water connections were not available to this site, all buildings are serviced by above ground holding tanks located below or adjacent to the serviced buildings. All holding tanks will be removed from site and existing surfaces made good where appropriate.

A number of surface water drainage modifications were carried out to suit the IBF operation, in particular the removal of kerb drains when the raised islands forming the car parking rows were removed and replaced with

linear drainage channels instead. All drainage modifications will be reinstated back to the original layout as part of the reinstatement works.

The reinstatement of the services can be summarised as follows:

Item	Reason for removal
Electrical connections to existing generators	Installed for IBF site operation, no longer required.
External Lighting Columns	Installed for IBF site operation, no longer required. Existing car park lighting to be retained.
CCTV Columns	Installed for IBF site operation, no longer required.
Foul Water Holding Tanks	Installed for IBF site operation, no longer required.
Surface Water Drainage	Adapted for IBF site operation, to be reinstated back to original design and layout.

2.3.3 Construction and Civils

A dedicated access/egress was created to the BTP compound with the existing one-way spine road changed to a two-way operation (due to the remainder of Car Park D being inaccessible). These changes will be reinstated back to the original layout and traffic movement around the site.

Some off-site adaptations to the immediate highway network were also carried out which mainly consisted of traffic management and line markings to International Way directly off the A2260 roundabout, all of which will be made good as part of reinstatement.

Within the site itself, a number of raised kerbed islands forming car parking rows were removed to accommodate lorry parking and circulation routes. These will be reinstated back to the original layout to both tarmac surfacing and vegetation including ornamental trees. A proposal for the reinstatement of the vegetation on a like for like basis can be found within Appendix A.

The perimeter boundary security fencing around the site remained largely in-situ with some minor modifications as previously noted above. A secondary heras fence was installed to some of the boundary lines, particularly along the Eastern boundary which will be removed as part of reinstatement. A permanent paladin weldmesh fence was installed to the boundary line between the IBF facility and the BTP welfare/car parking facility for security

reasons. This will be removed as part of the reinstatement works in order to fully maximise the car parking available for the landlord.

Existing tarmac surfaces disturbed by the installation of temporary structures, concrete slabs to support water tanks, cycle shelters, raised gate booths will all be made good as part of the reinstatement works. A number of linear drainage channels that serviced the car park have become damaged due to HGV movements, so these will need to be repaired/replaced with tarmac repairs as appropriate as part of reinstatement.

Timber fenced waste enclosures were constructed in the Southern and Western corners of the site and will also be removed, and the surfacing reinstated to its original construction.

The adaptations to the site line marking will be removed and the original layout will be reinstated using thermoplastic material as part of the resurfacing works.

The reinstatement of construction and civils work can be summarised as follows:

Item	Reason for removal
Adaptions to BTP welfare/parking	Installed to facilitate IBF site operation, no longer required. Access road to be reinstated back to one way and security fence removed.
Immediate Highway network line markings/traffic management.	Installed for IBF site operation, no longer required. Line markings and traffic management to be removed.
Surfacing & Kerbed Island Modifications	Modified for IBF site operation, to be reinstated.
Timber fence enclosers	Installed for IBF site operation, no longer required.
Line Marking	Reinstate to original layout.

2.4 Condition of Site at Conclusion of Reinstatement Work

The intended condition of the site following the reinstatement work will generally be as it was when HMRC first leased it. The exceptions to this are as detailed in Section 3 of this report and are namely:

1. Retention of Western boundary vehicle access onto site from International Way
2. Automated penstock valves to the surface water drainage system

A large yellow triangle with a white number 3 inside it, positioned in the upper right corner of the triangle.

3

Temporary Development to be
Retained

3.0 Temporary Development to be Retained

3.1 Introduction

The purpose of this section of the report is to explain and justify which elements of the temporary development are to be retained. This section of the report covers items that were approved under article 4(1) of the SDO.

3.2 Features to be Retained

3.2.1 Access from International Way

A number of modifications were carried out to the access and egress from the site to facilitate IBF site operations. A new entrance for HGV's was created along the Western boundary of the site off International Way including creating an opening in the boundary and adaptations to the highway and pedestrian footpath. To improve access into the car park once reinstated, the landlord has requested that this item be retained and not reinstated. The line markings on the immediate highway network would be removed to allow two lane traffic around International Way.

3.2.2 Automated Penstock Valves

As a condition of the original SDO, penstock valves were fitted to the existing surface water drainage system where it exits the site at two locations along the Eastern boundary. To provide ongoing pollution control of the site when it is reinstated back into its use as a car park, in agreement with the landlord it is proposed to retain these two valves in their existing locations.

3.3 Local Planning Authority Engagement

HMRC have carried out an engagement exercise with Ebbsfleet Development Corporation (EDC) acting in capacity as the local planning authority for the site. A copy of the response can be found within Appendix D of this report, however the key points are as follows:

- The proposed reinstatement of soft landscaping is welcomed, and it would be beneficial to have sight of a detailed landscaping proposal. A copy of the proposed landscape reinstated is appended to this report.
- No objection to the retention of the new vehicular access from International Way, as well as the retention of the automated penstock valve.

3.4 Landowner Engagement

HMRC have engaged with the landlord of the site HS1 Ltd throughout the reinstatement plan process to agree the level of development to be reinstated and which development features were to be included for retention as part of the plan. HS1 Ltd's agreement to the reinstatement plan inclusive of the elements of the scheme that are proposed to be retained is attached as Appendix D.

HMRC have also engaged with the landowner Ebbsfleet Development Corporation in both their capacity as the local planning authority and landowner of the site. A copy of the correspondence can be found within Appendix D as detailed within section 3.3 of this report.

Further correspondence has also been sought from landowner Kent County Council. A copy of the correspondence can be found within Appendix D.

4

Long Term SDO Approved Elements

4.0 Long Term SDO Approved Elements

4.1 Introduction

The purpose of this section of the report is to outline which, if any, of the physical elements of the Inland Border Facility (IBF) scheme approved by the SDO are intended for retention at the cessation of the IBF operation. This section of the report sets out the detail of these elements and provides justification for doing so, such as prior planning consents or where beneficial use outweighs removal.

4.2 Elements with SDO Approval for Retention

Not applicable

5

Article 4 Compliance

5.0 Article 4 Compliance

5.1 Introduction

The purpose of this section of the report is to outline where reinstatement follows that described in the Article 4 submission for the original SDO approval. In particular it identifies where reinstatement or retained features could give rise to significant environmental effects that were originally anticipated to be avoided. Where effects are envisaged they are assessed and justified.

5.2 Statement Detailing Article 4 Compliance

The following is based on the original Article 4 Analysis of Likely Environmental Effects of the Development (ALEED) submission which in each case predicted that there would be no likely significant effect. This is also the case with the planned reinstatement work.

Environmental Effect	Assessment and Justification
Air Quality	<p>The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There is the potential for the creation of dust particularly when reinstating or removing hardstand areas. The implementation of best practice construction methods will be adopted to control dust such as</p> <ul style="list-style-type: none"> • ensuring that all vehicles with open loads of duty materials are securely sheeted or enclosed. • Minimising stockpile heights and locate out of the wind • Enforcement of speed limits on surfaced/unsurfaced roads • Damp down in dry conditions • Vehicle engines and plant motors switched off when not in use
Cultural Heritage	<p>The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. No new excavations are proposed that would potentially impact on any unknown archaeological remains. Temporary impacts to the setting of nearby heritage assets from reinstatement activities would be minor and the effects would not be significant. No permanent impacts on heritage assets are anticipated and as a result no additional mitigation measures are required.</p>

Landscape and Visual Effects	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. Vegetation that was removed will be replaced on a like for like basis with the exception of the hedgerow and amenity grassland removed in order to form the access from the Western Boundary. This was noted to be a defunct species-poor hedge in the original SDO submission and given the extent of removal was minimal when compared to the overall boundary vegetation this not deemed have a significant impact on biodiversity.
Geology and Soils	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. All excavations will be in areas previously disturbed during the construction so no significant effects are anticipated during reinstatement. However, best practice measures will be adopted during reinstatement including the provision of spill kits with reinstatement staff trained in their correct application.
Biodiversity	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. Vegetation that was removed will be replaced on a like for like basis with the exception of the hedgerow and amenity grassland removed in order to form the access from the Western Boundary. This was noted to be a defunct species-poor hedge in the original SDO submission and given the extent of removal was minimal when compared to the overall boundary vegetation this not deemed have a significant impact on biodiversity.
Material Assets and Waste	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There will be waste produced as part of the reinstatement works so in order to reduced any non-significant effects best practice measures such as the principles of the waste hierarchy are adhered to including the prevention, reuse and preparation for re-use, recycling, recover and disposal of waste. Opportunities will be explored for the temporary structures (offices and inspection sheds) to be sold and re-used elsewhere where they are not leased.
Noise and Vibration	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There is the potential for a temporary adverse effect on nearby sensitive receptors as a result of noise arising from the works associated with the reinstatement such as the dismantling of the temporary structures. As this is small scale in nature and of a temporary duration no significant effects are anticipated, however best practice measures to minimise noise and noise limits as set out in BS5228-1 will be followed by the reinstatement contractor.
Population and Human Health	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all

	temporary structures removed. No road or public rights of way closures are proposed and there would be no restrictions on local businesses as part of the reinstatement. There will be some slight disturbance from construction machinery and noise as part of reinstatement however it is proposed that the local community will be informed of the proposals for reinstatement including approximate timescales, as well as controlling HGV movements on site by appropriate site marshals.
Road Drainage and Water Environment	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. There are potential effects to the water environment during reinstatement due to existing pollution pathways, however it is proposed that the existing penstock valve would remain to reduce any potential. In addition, best practice guidance will be followed in line with guidance CIARA (2001).
Climate	The reinstatement of the scheme is not anticipated to result in any new or materially different effects than the construction of the scheme as the site will be restored to its original state with all temporary structures removed. Opportunities will be explored for the temporary structures (offices and inspection sheds) to be sold and re-used elsewhere where they are not leased.

For specific references to the relevant lines of the REAC see Section 22 of this document.

6

Plans and Drawing References

6.0 Plans and Drawing References

6.1 Introduction

The purpose of this section of the plan is to detail the documentation describing the planned reinstatement work across the site.

6.2 Plans and Drawing References and Descriptions

Drawings can be found in Appendix A of this report.

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7

Programme

7.0 Programme

7.1 Introduction

The purpose of this section of the report is to detail the programme for the reinstatement work which demonstrates that it will be completed prior to the expiration of the SDO permission period.

7.2 Programme

A programme can be found in Appendix B of this report.

8

Contractor Details

8.0 Contractor Details

8.1 Introduction

The purpose of this section of the report is to provide details of the appointed contractor carrying out the reinstatement works on behalf of the Site Operator, HMRC.

8.2 Contractor Details

The contractors to undertake the reinstatement work are yet to be appointed by HMRC. This will be subject to a competitive tendering exercise in accordance with The Public Contracts Regulations 2015 and OJEU (depending on the final financial threshold of the reinstatement works).

9

Site Monitoring

9.0 Site Monitoring

9.1 Introduction

The purpose of this section of the document is to provide details of any monitoring of the site and reinstatement work to ensure that the target condition of the site is achieved.

9.2 Site Monitoring

HMRC and their appointed project manager, will be undertaking regular site visits to ensure quality and compliance is maintained. The appointed contractor will also be requested to provide regular progress reports and attend site meetings.

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10

Post Reinstatement Maintenance

10.0 Post Reinstatement Maintenance

10.1 Introduction

The purpose of this section of the document is to provide a report detailing how the site will be maintained post reinstatement, and by whom.

10.2 Post Reinstatement Maintenance

The site will be handed back to the Landlord, HS1 Limited once the reinstatement works have been completed and the lease has ended. HS1 Limited will revert back to having ongoing responsibility for the site. Ongoing Soft Landscape Maintenance will be carried out by HS1 Limited in accordance with the details set out in the Soft Landscape Proposals as detailed within Appendix A.

11

Operational Management Plan & Construction Management Plan

11.0 Operational Management Plan and Construction Management Plan

11.1 Introduction

The purpose of this section of the plan is to describe where there are overlaps how the proposals align with commitments made in the Operational Management Plan (OMP) and Construction Management Plan (CMP) which were submitted as part of Article 4 of the SDO application.

11.2 Construction Management Plan

The approved Construction Management plan referenced below, dated 30th November 2020 was submitted as part of the Article 4 requirements of the SDO 2020. This plan includes the relevant methodologies for mitigating impacts on noise and air quality etc that could arise from the original construction of the Ebbsfleet IBF site. The same mitigations apply to any reinstatement work. The approved Construction Management Plan is available for review upon request from HMRC.

The requirements of the Construction Management Plan relate to the SDO approved construction work to create the Inland Border Facility Site. The Reinstatement Work will be in accordance with this plan to ensure that those requirements, which were approved for the SDO, are maintained during the subsequent construction work required to reinstate. The SDO requirements of the Construction Management Plan (CMP) are outlined below:

No works of construction may commence until a construction management plan for the development has been submitted to and approved by the Secretary of State. The plan must comprise details of policies and procedures to be complied with in connection with the construction of the development in relation to:

- a) engagement with relevant owners and occupiers, including complaints handling.*
- b) application of best practicable means to minimise noise, vibration and emissions to air.*
- c) application of best practicable means to minimise the adverse effects of lighting on the amenity of relevant owners and occupiers, ecological receptors and road users.*
- d) sourcing, placing, managing and storing of construction materials, including, where appropriate, the stripping, storage and re-spreading of soil.*
- e) management of waste in accordance with the waste hierarchy.*
- f) pollution prevention and control*
- g) preventing damage to trees to be retained on the site, or trees immediately adjacent to any works on the site.*

h) management of construction traffic, including measures to prevent the deposit of mud and construction materials on the highway.

i) the management of invasive species. and

j) where appropriate, the appointment and retention of a suitably qualified archaeologist, arboriculturist, ecologist or ordnance specialist to oversee works

The work involved to reinstate the Ebbsfleet IBF site, as described elsewhere in this plan, is relatively small scale so as a result those items detailed in the Construction Management Plan above will be minimal.

A response to each item:

- a) See Section 12 of this document.
- b) See Section 16 of this document.
- c) The lighting specific to the IBF will be removed, and the existing car park lighting that was left in-situ will be powered back on.
- d) Few materials will be required for the reinstatement plan and where they are they will be delivered and stored on the site. Storage of these materials will be in accordance with the provision of the CMP:
 - Storage of any oil-based materials, including petrol, diesel, waste and vegetable and plant oil, and above ground fuel and oil storage tanks, the Principal Contractor will comply with the Control of Pollution (Oil Storage) (England) Regulations 2001, as amended, and the Environment Agency's PPG: Above ground oil storage tanks
 - Chemical storage, handling and use will comply with PPG 26: Drums and intermediate bulk containers
 - Stationary plant will be used with secondary containment measures such as plant nappies to retain any leakage of oil or fuel, which will be emptied at regular intervals to prevent overflow
 - As per CIRIA guidance, fuel will be stored in dedicated bunded, impervious storage areas at least 10m away from any water body including drains and watercourses.
 - Fuel tanks will be stored within a bund capable of holding 110% of their capacity
 - Stockpiles and mounds will be kept away from sensitive receptors (including natural and historic features), watercourses and surface drains where reasonably practicable, and sites to take into account the predominant wind direction relative to sensitive receptors
 - Stockpiles and mounds will be maintained to avoid materials slippage
 - Materials stockpiles likely to generate dust will be enclosed or securely sheeted, kept watered or stabilised as appropriate
 - Fine dry material will be stored inside buildings or enclosures with measures in place to ensure no escape of material and of overfilling during delivery

- Maintain a clean and tidy site, ensuring all material is stored in the correct areas
- e) See Section 14 of this document.
- f) Due to the small-scale nature of the reinstatement work the levels of pollution will be far lower for this work than previously anticipated for the use of the IBF site.
- g) Where works are to be carried out within close proximity of existing trees on or near the site, protective barriers will be installed in accordance with BS5837:2012, at the distances dictated by the root protection area.
- h) See Section 15 of this document.
- i) Due to the small-scale nature of the reinstatement work and disruption to natural habitats this is not anticipated to be a factor.
- Should any protected species be found during the works, works will stop immediately, and an ecologist contacted immediately via the Project Manager on-site.
 - Should invasive species such as Japanese Knotweed be encountered on-site, works will stop immediately, and the area fenced off. An ecologist will be notified via the Project Manager on-site for further action.
- j) See Sections 19 and 20 of this document.

11.3 Operational Management Plan

The requirements of the Operational Management Plan relate to the SDO approved operation of the Inland Border Facility Site. The Reinstatement Work will generally be in accordance with this plan to ensure that those requirements, which were approved for the SDO, are maintained during the subsequent construction work required to reinstate. The SDO requirements of the Operational Management Plan (OMP) are outlined below:

No use other than works of construction and tests of the operation of the development may commence until an operational management plan for the development has been submitted to and approved by the Secretary of State. The plan must comprise details of policies and procedures to be complied with in connection with the use and operation of the development...

- a. Engagement with relevant owners and occupiers, including complaints handling*
- b. Dealing with adverse weather, incidents and protestors*
- c. Emergency response*
- d. Inspection, maintenance and repair of hard surfacing, surface and foul water drainage systems, fire hydrants and emergency water supplies*
- e. Managing traffic associated with the operation of the development, including:

 - i. the management of vehicles moving between the site and the SRN (being the highways for which Highways England is the highway authority, by virtue of article 2 of the Appointment of a Strategic Highways Company Order 2015(b)), and the provision of signage for their drivers and*
 - ii. preventing vehicles carrying goods specified in paragraph A.4 of Part 1 from having access to, or being stationed on, the site**
- f. Pollution prevention and control*
- g. Prescribing limits on levels of noise and emissions to air that will be adhered to during the operation of the development, and monitoring and management measures to secure adherence to those levels*
- h. Managing waste and waste disposal in accordance with the waste hierarchy*
- i. A local employment strategy for staff*
- j. Managing and enhancing biodiversity*

As per section 11.2, the work involved to reinstate the Ebbsfleet IBF site, as described elsewhere in this plan, is small scale and so impact to the items outlined in the Operational Management Plan above will be minimal.

A response to each item:

- a) See Section 12 of this document.

- b) Not applicable.
- c) Not applicable.
- d) Not applicable.
- e) Not applicable but for construction traffic management see Section 15 of this document.
- f) Due to the small-scale nature of the reinstatement work the levels of pollution will be far lower for this work than previously anticipated for the use of the IBF site.
- g) See Section 16 of this document.
- h) See Section 14 of this document.
- i) Not applicable.
- j) Due to the small-scale nature of the reinstatement work and disruption to natural habitats this is not anticipated to be a factor.
 - a. Should any protected species be found during the works, works will stop immediately, and an ecologist contacted immediately via the Project Manager on-site and the appropriate action as advised by the ecologist will be taken.
 - b. Should invasive species such as Japanese Knotweed be encountered on-site, works will stop immediately, and the area fenced off. An ecologist will be notified via the Project Manager on-site for further action and the appropriate action as advised by the ecologist will be taken.

12

Engagement with Relevant Owners and Occupiers

12.0 Engagement with Relevant Owners and Occupiers

12.1 Introduction

The purpose of this section of the document is to outline where community engagement for the proposals of the Reinstatement Plan has been necessary and undertaken. This section outlines who the relevant owners and occupiers are and how they could potentially be impacted by the planned reinstatement work.

Additionally, relevant statutory bodies (e.g. Environment Agency, Natural England) will be consulted where appropriate to do so.

Finally, a complaints handling procedure is set out to detail how issues can be resolved if this isn't possible at a site level.

12.2 Engagement with Relevant Owners and Occupiers

The scale of the reinstatement work identified in this plan are minimal and time on site is to undertake this work has been estimated at 18 weeks, and those neighbouring stakeholders are likely to only experience minimal traffic for the work and large plant will only be required for the repairs/reinstatement to the existing hardstanding areas to bring the site back into use as a car park.

The landlord of the site is High Speed 1 Limited (HS1) and other than disused scrubland and an authorised landfill site, HS1 are the nearest landowner to the IBF site. There have been no other owners or occupiers identified in the immediate surrounding area.

Considering the extent of reinstatement work planned and it has not been deemed necessary to involve any statutory bodies, however HMRC will engage with the local authority, Dartford Borough Council in relation to the scope and timing of the reinstatement.

The complaints handling procedure will be as agreed in the Article 4 submission of the SDO and as detailed below:

1. A sign will be placed in a prominent location close to the entrance of the IBF which will clearly provide details of how to make contact with the site and the contractors undertaking the reinstatement work on behalf of HMRC.

2. The email address for the site will be monitored while the site is occupied.
3. Complaints which are related to the reinstatement of the site will be received formally, either in writing, such as by email or letter, or verbally by phone or in person, and acknowledged upon receipt where possible (by email auto response, or an automated voicemail) or within a maximum of two business days (to manually issue a complaint reference number).
4. Details of all complaints received at the site and by whom will be noted by the member of staff who the complaint is made to, who will notify their line manager immediately, to ensure all required details have been captured.
5. Messages of complaint received at the site will always be recorded on an incident log. Verbal or in person complaints will be initially handled by the stakeholder's line manager who will endeavour to resolve the complaint at site level, at the time at which it is received. If a complaint cannot be easily resolved, then the complaint will be logged into the Complaints Database and a Complaints number issued to the person making the complaint. The issuing of a complaints number is intended to be taken as formal acknowledgement of the complaint.
6. Complaints received in writing or by email will be addressed by HMRC and it is their responsibility upon receipt to ensure all complaints are recorded.
7. HMRC will be responsible for assessing the complaint as to its urgency, implications, and relevance to the site or to a Government Agency. If HMRC are unable to resolve the complaint at the time it is received, then they will advise the complainant of the eight business day timescale for a final response to be reached and provide them with a complaint reference number (if the complainant does not already have one)– which is intended to be taken as formal acknowledgement of the complaint.
8. Complaint responses will be handled by HMRC, who will triage, identify owners, and seek feedback from relevant stakeholder before sending a response to the complainant.
9. HMRC aim to have a final resolution to all complaints received within eight business days. Should a final response not be possible within the target period, such as an extended investigation or awaiting statements, the complainant will be contacted by HMRC within eight days to advise a revised timescale for resolution.
10. Each complaint will be handled fairly, confidentially and the issues will be investigated thoroughly before issuing the final response.
11. If a final response to a complaint been issued by HMRC but the complainant does not agree with the resolution determined by the outcome of the internal investigation, the complainant is able to raise an appeal. A senior leader from HMRC will review the complaint for a second time and respond within three working days. The complaint will again be handled fairly, confidentially and the issues will be investigated thoroughly before issuing another final response.
12. If the complainant remains unsatisfied with the resolution offered by the internal appeal process, then the final avenue for escalation is to write to the local MP and ask for the matter to be referred to the Parliamentary Ombudsman. The complainant will be signposted to the local MP and the central database will capture actions taken.

13. It is possible that a complaint will be made verbally to any member of staff on-site (e.g. to a member of security staff at the entrance/exit to the site or by phone/email), therefore the following standards are required to be followed by staff when a complaint is being made to them:
- a. The recipient of the complaint will calmly listen to what is being said and remain objective. After hearing the complainant's full account of events, the recipient will reiterate the main points of the complaint back to the complainant to ensure all relevant details have been captured.
 - b. The complainant will not be challenged on the facts of what they are saying, listening and acknowledging what is being said is a key requirement, and the complainant must be thanked for getting in touch. The complainant must be asked what their expectations are in terms of what they feel would be an acceptable resolution, and this will be recorded by the recipient. However, to manage the complainant's expectations it must be made clear that while all effort will be made to resolve the complaint to the complainant's satisfaction, the resolution put forward by them may not necessarily be the one determined by the complaint investigation outcome.
 - c. Recipients are to take every complaint made seriously, they will record the complainant's details (name and preferred method of contact) and advise the complainant that the matter will be investigated in full.
 - d. The recipient will notify their line manager immediately that a complaint has been made to them and pass on all details they captured regarding the complaint.

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13

Site Safeguards

13.0 Site Safeguards

13.1 Introduction

The purpose of this section of the document is to detail any measures that will be used to protect any existing biodiversity on the site, and / or any biodiversity enhancement measures that have been agreed as part of the Biodiversity Enhancement Plan for the IBF (if any). This section will also outline the measures that will be put in place, if required, to ensure that during site reinstatement, biodiversity enhancements created alongside the provision of IBF sites will be protected.

13.2 Site Safeguards

As detailed in the original Article 4 submission, no significant effects are anticipated upon biodiversity from the decommissioning of the scheme. As the vegetation that was removed during construction is no longer present, no specific mitigation measures are required during the reinstatement. Vegetation that was removed to accommodate the construction of the IBF is to be replanted on a like for like basis supported by an appropriate planting specification, as detailed within Appendix A of this report.

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14

Waste & Reuse of Materials

14.0 Waste & Reuse of Materials

14.1 Introduction

The purpose of this section of the document is to demonstrate the extent to which materials will be re-used and recycled off site. Where material is not being recycled details are provided for how they are being disposed of and how doing so meets the relevant industry standards such as the Waste and Resource Action Programme (WRAP).

14.2 Waste & Reuse of Materials

Any hardstanding or tarmac surfacing that is proposed to be planed off as part of any repair or reinstatement works will be reused. This material will be removed from site by the appointed contractor's specialist subcontractor. Road chippings and hard core have value and so will be taken away from site to be recycled/reused elsewhere.

All temporary accommodation, fixtures and fittings will be removed and used elsewhere. Where feasible HMRC are exploring using these items on other IBF sites, if this is not possible the units are generally leased/on hire and will be used by the supplier on other sites.

Skips and other storage receptacles used for the containment of construction, demolitions and excavation waste will be colour-coded in line with generic scheme developed by the Institution of Civil Engineers.

Hazardous waste and non-hazardous waste must be stored separately. The mixing of hazardous and non-hazardous waste, either whilst stored on-site or upon collection will not be permitted.

All skips and other storage receptacles will have appropriate signage to facilitate separation of waste for re-use, recycling, or disposal. Plastic sheeting will be used to prevent leaching from waste soils and aggregates where these are not contained within skips or other storage receptacles.

All skips and storage receptacles will be sheeted, or otherwise remain lidded or closed, when waste is not being deposited into them. They will also be covered to prevent the escape of waste whilst in transit and loaded for maximum payload efficiency.

All skips and storage receptacles will be inspected on arrival to ensure they are fit for purpose. Any not fit for purpose will be taken out of use immediately with appropriate signage to signify it will not be used

Liquid waste will be stored on hard-surfaced areas with secondary containment systems to prevent spillages.

Waste will not be stored within 10m of any controlled watercourse, borehole, well, spring, surface water drainage system or foul water drainage system.

15

Site Traffic and Considerate Construction Protocols

15.0 Site Traffic and Considerate Construction Protocols

15.1 Introduction

The purpose of this section of the report is to demonstrate how site traffic and other construction protocols will meet the requirements of the approved Construction Management Plan (CMP) advice note.

15.2 Site Traffic and Considerate Construction Protocols

Site traffic for reinstatement will be considerably reduced in comparison to the approved SDO use for HGV movement. Plant required on site will include those typically used for resurfacing works and HGVs to cart away waste from the disposal of road planings as part of any resurfacing and repair work. Other anticipated vehicles include cranes and hiab vehicles to remove the temporary structures from site, and vans for operatives to travel to site in with their tools and materials for disconnecting services and making good surfaces.

16

Noise and Air Quality

16.0 Noise and Air Quality

16.1 Introduction

The purpose of this section of the report is to demonstrate that the relevant noise and air quality objectives, as detailed in the Construction Management Plan (CMP) and Operational Management Plan (OMP), are not breached during the reinstatement work.

16.2 Noise and Air Quality

Any noise and air pollution during the reinstatement of this site will be drastically reduced from what was approved for the use of the site as a temporary Inland Border Facility.

For the limited number of vehicles that will be required the following will be adhered to:

- The use of mains electrical power, hybrid generator systems and battery powered equipment where reasonably practical
- All plant on-site will be in good operating condition. Exhaust emission from all plant will be reduced as much as possible through effective and regular maintenance
- All construction vehicles will conform to the current EU emissions standards (NRMM Stage V compliance for all plant with an engine capacity of between 37-560kW and all generators above 560kW)
- All plant to be switched off when not in use
- No idling of vehicles
- Low sulphur diesel fuel will be used in all plant (non-road mobile machinery)
- The contractor will maintain a register of all plant on-site to record inspection and set up a system of regular inspection of all plant
- Vehicle and construction plant exhausts will be directed away from the ground and position them at a height to facilitate appropriate dispersal of exhaust emissions

17

Soil Handling for Site Reinstatement

17.0 Soil Handling for Site Reinstatement

17.1 Introduction

The purpose of this section of the report is to detail, where required, how conserved soils will be used for the restoration of land to its previous use, or subsequently agreed use. This includes setting out how soils will be handled, stored and used during site reinstatement.

17.2 Soil Handling for Site Reinstatement

Not applicable.

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18

Standards

18.0 Standards

18.1 Introduction

This section of the report addressed the relevant British Standards that any reinstatement work should adhere to, in particular those related to planting and trees.

18.2 Standards

The following standards are mainly applicable to the resurfacing works required:

- BS EN 13108 Bituminous mixtures. Material specifications
- BS 594987 Asphalt for roads and other paved areas. Specification for transport, laying and compaction and type testing protocols
- BS EN 12697 Bituminous mixtures. Test methods for hot mix asphalt

A large yellow triangle pointing downwards, positioned on the left side of the page. The number 19 and the word Monitoring are placed to its right.

19

Monitoring

19.0 Monitoring

19.1 Introduction

The purpose of this section of the document is to set out the proportionate measures and arrangements to monitor and manage the reinstatement work at this site. If relevant details are included detailing arrangements for additional work as may be required that are outside of the current scope to ensure the site achieve the target condition.

If required this section also provides detail for any post-handover monitoring, the party responsible, the extent of any liabilities and timescale for ongoing monitoring.

19.2 Monitoring

Have been appointed to project manage the reinstatement of this site. The process will be monitored by them and HMRC to ensure that timely progress is made by the appointed contractor so that they meet the programme.

Time has been allowed in the programme for any unforeseen work that may be required on this site to reinstate it.



20

Supervision

20.0 Supervision

20.1 Introduction

The purpose of this section of the document is to provide details of the suitably qualified and experienced personnel who will be responsible for supervising the reinstatement work, outside of the principal contractor role.

20.2 Supervision

Have been appointed to project manage this process and senior chartered surveyors will be responsible for supervising the reinstatement of this site.

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21

Handover

21.0 Handover

21.1 Introduction

The purpose of this section of the document is to provide details of who the reinstated site will be transferred to upon completion of the reinstatement works. If required, this section will also detail the period of time for which the contractor is responsible for replacing any landscape planting (e.g. trees and shrubs) which may fail after initial planting.

21.2 Handover

The site will be handed back to the Landlord, HS1, upon completion of the reinstatement work and termination of the lease. HS1 will be responsible for ongoing maintenance of the site.

22

Other Site Specific Requirements as
Detailed in Advice Note

22.0 Other Site Specific Requirements as Detailed in Advice Note

22.1 Introduction

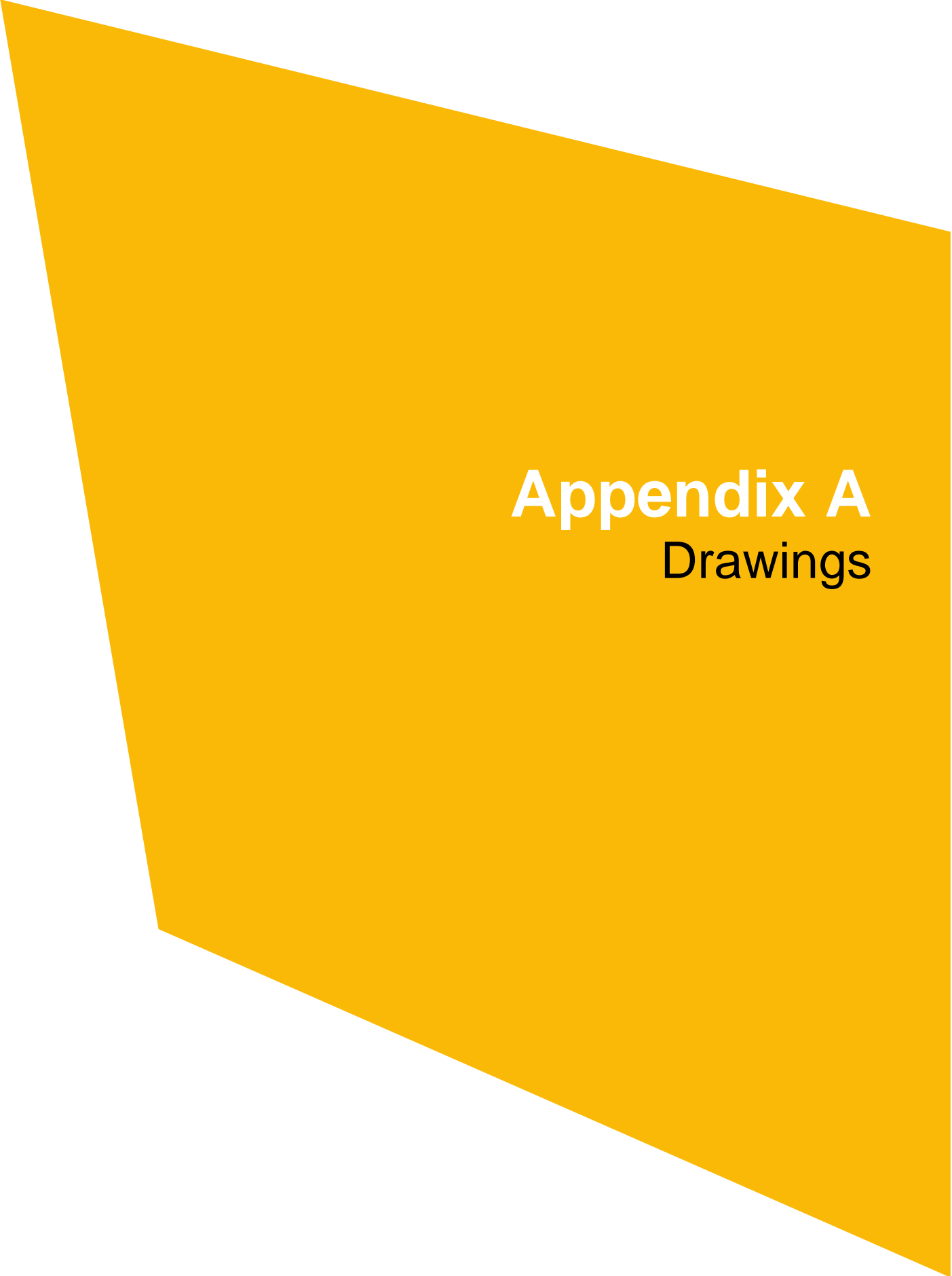
This section of the document outlines the measures detailed in the REAC which are required to be addressed as part of the Reinstatement Plan. Full details can be found within Appendix C.

22.2 Other Site Specific Requirements

The requirements from the REAC for the items relevant to the Reinstatement Plan are as follows:

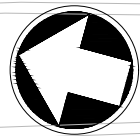
Condition	Response
AQ1	<p>It is not anticipated that the removal of the temporary structures will create significant dust emissions during reinstatement, however the following best practice measures will be adopted by the contractor:</p> <ul style="list-style-type: none"> • Minimising stockpile heights and profiles to limit wind-blown dust emissions • Vehicle with open loads to be securely sheeted or enclosed • Maximum speed limit on site to prevent the generation of dust by fast moving vehicles • Damp down surfaces in dry conditions • Vehicle engines and plant motors to be switched off when not in use
L1	<p>During reinstatement the contractor is to ensure that the site is well managed and tidy at all times, governed by an appropriate construction phase plan for the reinstatement works.</p>
L2	<p>The following measures will be adopted in order to protect the RPA of any affected trees:</p> <ul style="list-style-type: none"> • Protective barriers installed in accordance with BS5837:2012 with guidance from a qualified Arboriculturalist • Vehicle movements to remain on hardstanding at all times • Contractor compound and set down areas to be positioned on hardstanding • Any roots discovered during reinstatement works to be pruned/retained in accordance with good practice
L4	<p>The vegetation removed as part of the adaptations to the site access is to be reinstated on a like for like basis supported by an appropriate planting specified from a suitably qualified landscape architect as detailed within Appendix A, save for the small section of hedgerow and amenity grassland as detailed in Section 5 of this report.</p>

M1	The reinstatement contractor is to ensure that the principals of the waste hierarchy is followed when dealing with was on site including prevention, reuse and preparation for reusing, recycling, recover and disposal.
NV1	The reinstatement contractor is to ensure all noisy operations required as part of the reinstatement would be completed between 0800 and 1800 on weekdays and 0800 to 1300 on Saturdays. All noise emitting equipment will be switched off when not in use.
NV2	<p>The following noise mitigation measures will be implemented as part of the reinstatement works:</p> <ul style="list-style-type: none"> • Equipment to be maintained and in good working order • Fitting equipment with silencers or muddlers where appropriate • Management of deliveries to prevent unnecessary queuing traffic into site • Lowering of materials instead of being dropped from a height • Use of adjustable or directional audible vehicle reversing alarms
PH1	HMRC are to ensure that the local community are informed of the reinstatement proposals through their existing communication channels such as providing details on a website, site notices and letter drops where appropriate.
RDWE1	<p>As part of the reinstatement works the contractor will manage the works in accordance with the CIRIA guidelines on best practice in relation to water management such as:</p> <ul style="list-style-type: none"> • Reinstatement workers to be briefed on the use of spill kits as part of their site induction • All plant and machinery to be maintained in a good condition • Pollution prevention and spill response procedures to be developed by the contractor • Dust suppression in accordance with AQ1 of the REAC as above • Retention of penstock valve
C1	<p>The contractor is to reduce carbon emissions as part of the reinstatement works by including the following within their working practices:</p> <ul style="list-style-type: none"> • Transportation of materials to site using low-carbon modes where possible • Use of low carbon construction materials required for the reinstatement • Provision to effectively segregate waste during the reinstatement works
C3	As part of the reinstatement opportunities will be explored for the temporary structures (offices and inspection sheds) to be sold and re-used elsewhere where they are not leased or rented.

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Appendix A

Drawings



Key to symbols			
	Staff Parking		Cycle Parking
	Examination facility		Examination Offices
	HMRC Office		Marshalls Facility
	Drivers Facility		Quarantine Bay
	Preliminary Inspection Bay		Refuse Vehicle Bay
	Delivery Vehicle Bay		Raised Gate Booth
	Access / Egress Cabin		Smoking Shelter
	Marshal's Shelter		Hazardous waste Storage
	Bin Storage		Grit Bin
	Spill Kit / Fire Extinguishers		Safe walking routes
	Fire Water System		
	Fire Alarm Unit - Manual call point, sounder and flashing beacon		
	Emergency Rendezvous Point		
	Concrete Barrier (3m x 0.45m x 0.8m)(LxWxH)		
	Fencing		
	BTP area - excluded from red line boundary		

Rev.	Description	By	Date
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Client



Project

Ebbsfleet IBF
Reinstatement Plan

Drawing Title

Existing Plan

Drawing No. NTBS3728/01

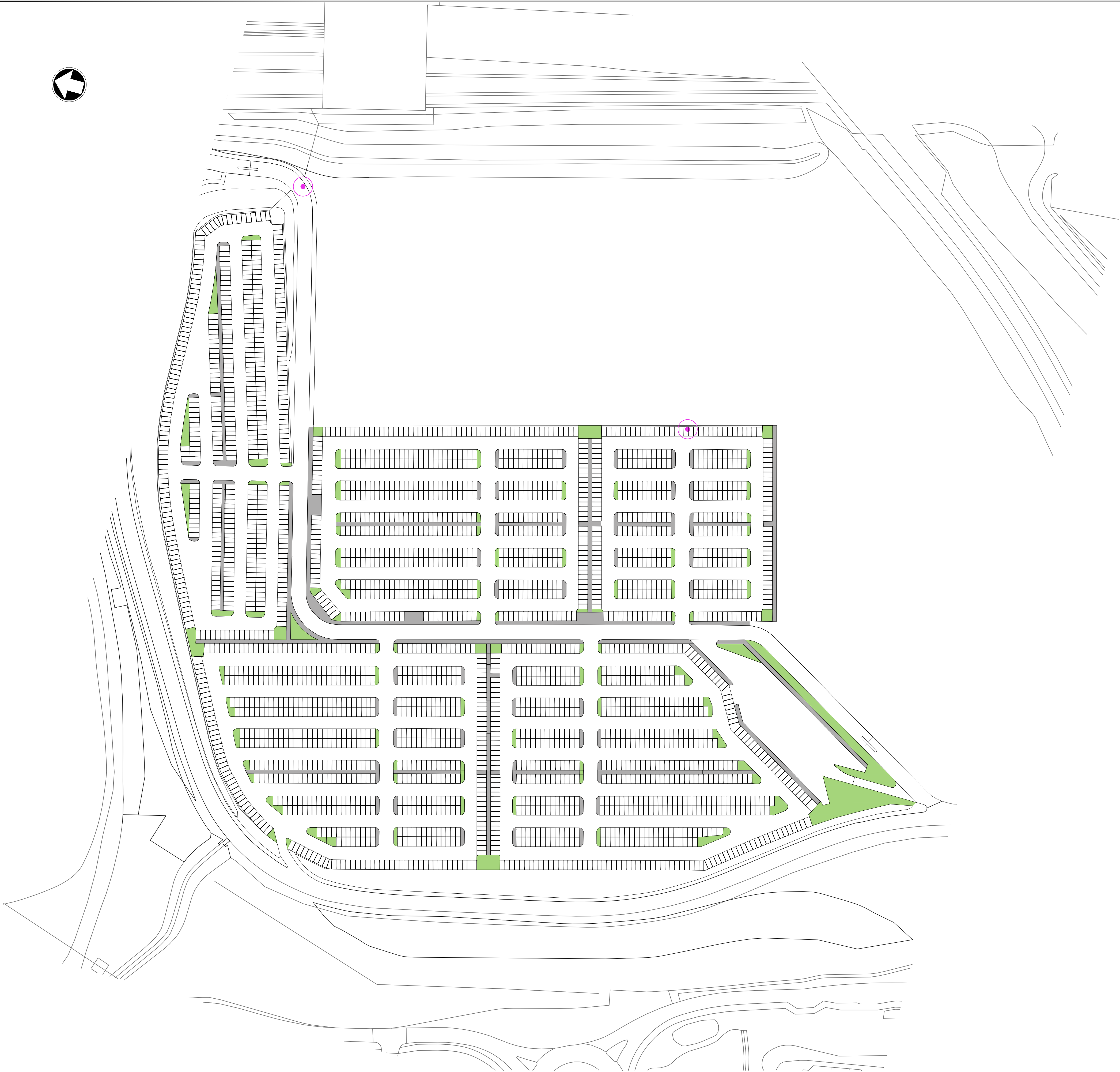
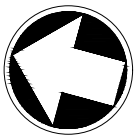
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Drawing Status

Drawn Checked

Date 04/2022 Rev.





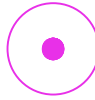
KEY



Landscaped Island / Feature



Tarmaced Islands



Automated Penstock Valves

Rev.	Description	By	Date
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Client



Project

Ebbsfleet IBF
Reinstatement Plan

Drawing Title

Proposed Plan

Drawing No. NTBS3728/02

Scale 1:1000 Original Sheet Size A1

Drawing Status

Drawn Checked

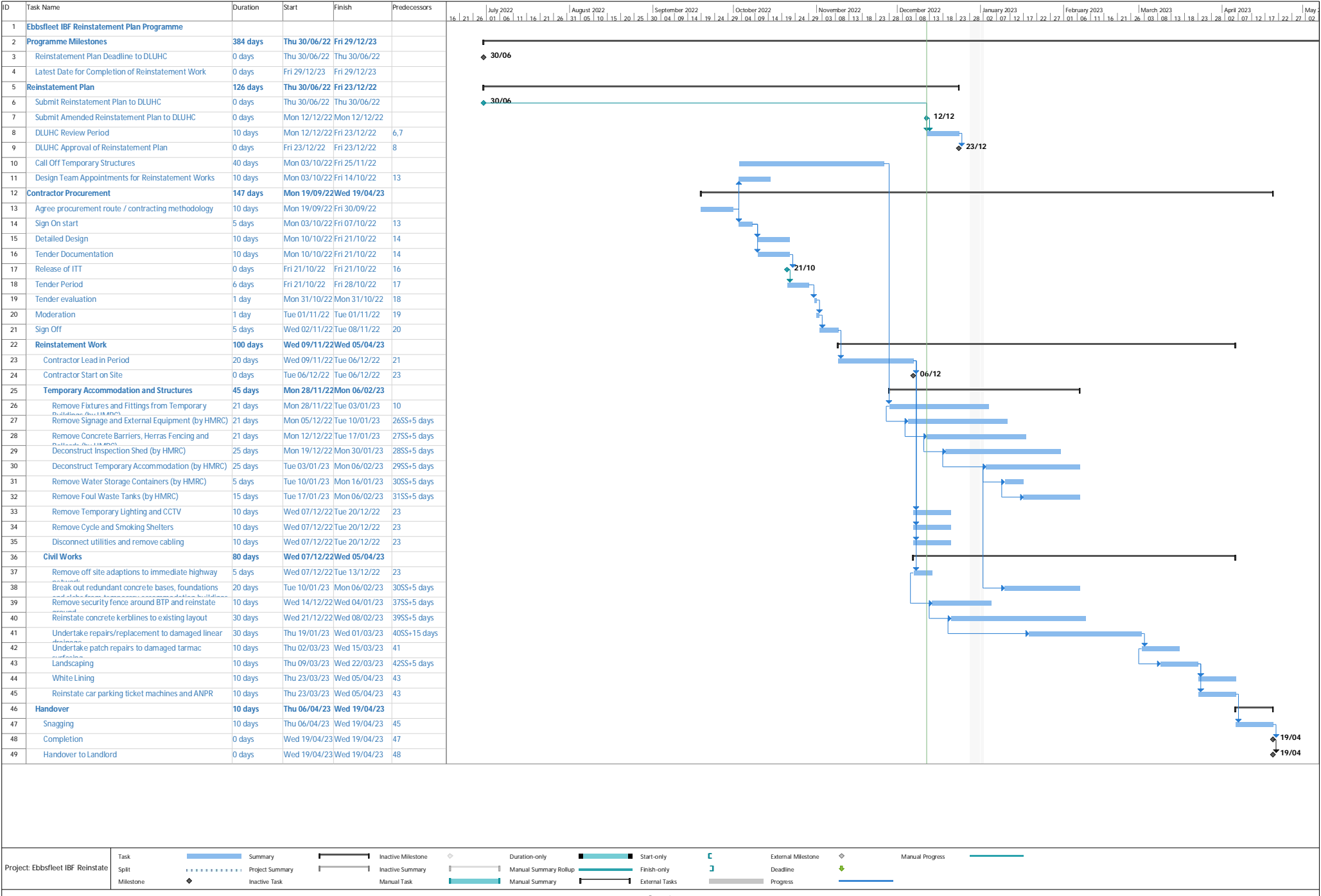
Date 04/2022 Rev. B



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Appendix B

Programme



Appendix C

Record of Environmental Actions and Commitments (REAC)

Table C.1: Record of Environmental Actions and Commitments

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
General (G)						
G1	Reduce light disturbance for sensitive receptors	Operation	<p>The following measures would be required during construction and operation if additional lighting is required on the site:</p> <ul style="list-style-type: none"> The main beam angle of any artificial lighting must be directed downward so as to minimise light spill. Lighting would be at the minimum luminosity necessary and use low energy consumption fittings. Where appropriate, lighting would be activated by motion sensors to prevent unnecessary usage. It would comply with the Institute of Lighting Professionals Guidance Notes for the Reduction of Obtrusive Light GN01⁶ and the provisions of BS 5489, Code of practice for the design of road lighting⁷, where applicable. 	No	Incorporation within lighting design	Principal Contractor/Operator
Traffic and Transport (TT)						
TT1	Support the operation of the site	Operation	<p>The following plans would be produced with relevant mitigation measures implemented to minimise the impacts on traffic in the local area:</p> <ul style="list-style-type: none"> Traffic Management Plan Signage Strategy and Staff Travel Plan 	No	Incorporation within the OMP	Principal Operator
Air Quality (AQ)						
AQ1	To limit and control dust emissions during construction	Construction and Reinstatement	<p>Works would be carried out in accordance with Best Practicable Means, as described in Section 79 (9) of the Environmental Protection Act 1990, to reduce the creation of dust on site during the construction phase. This would include:</p> <ul style="list-style-type: none"> Minimise height of stockpiles and profile to minimise wind-blown dust emissions and risk of pile collapse. 	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor

⁶ Institute of Lighting Professionals (2011) Guidance notes for the reduction of obtrusive lights [online] available at: <https://www.theilp.org.uk/documents/obtrusive-light/> (last accessed December 2018).

⁷ British Standards Institution (2013) BS 5489, Code of practice for the design of road lighting. Lighting of roads and public amenity areas [online] available at: <https://shop.bsigroup.com/ProductDetail/?pid=000000000030217237> (last accessed December 2018).

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
			<ul style="list-style-type: none"> • Locate stockpiles out of the wind (or cover, seed or fence) to minimise the potential for dust generation. • Ensure that all vehicles with open loads of potential dusty materials are securely sheeted or enclosed. • Enforce a maximum speed limit of 15mph on surfaced roads and a 10mph speed limit on unsurfaced haul roads and work areas, to prevent the generation of dust by fast moving vehicles. • Damp down surfaces in dry conditions. • All vehicle engines and plant motors shall be switched off when not in use. 			
Landscape (L)						
L1	To limit visual intrusion and impacts upon landscape character	Construction and Reinstatement	<p>The following measures to be undertaken to reduce visual intrusion and impacts upon the landscape:</p> <ul style="list-style-type: none"> • Keep a well-managed and tidy site. • Use of directional, hooded and low-level lighting, as well as restrictions on night-time lighting during construction. 	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor
L2	Ensure no effects to RPA	Construction and Reinstatement	<p>The following measures would be required:</p> <ul style="list-style-type: none"> • Protective barriers should be installed in accordance with BS5837:2012 around all of the trees in close proximity to the works, at the distances dictated by the RPA measurements. • An Arboriculturalist should attend site to confirm the final positioning of the protective fencing. • Vehicular movement should remain within hardstanding at all times. • Compound areas and set down areas were not confirmed at the time of the site visit; however, it is recommended that they should be positioned on an area of hardstanding within the site. If hardstanding is not available, they should be located outside of the RPAs of any trees on site. 	No	Incorporation within CMP and Reinstatement Plan Arboricultural Clerk of Works on site	Principal Contractor and Reinstatement Contractor

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
			<ul style="list-style-type: none"> Any roots <25mm diameter which are exposed by the excavations are to be pruned properly in accordance with good practice using secateurs or a sharp saw; No roots >25mm are to be pruned or severed without prior agreement from the scheme Arboriculturalist. Exposed roots >25mm should be retained and covered with moist hessian until they are reburied. If roots >25mm are uncovered, the scheme Arboriculturalist should advise on whether the trees stability and integrity have been compromised and decide on appropriate action required. Any works relating to installation of services must be undertaken in accordance with the National Joint Utilities Group Guidelines for the Planning, Installation and Maintenance of Utility Apparatus in Proximity to Trees. 			
L3	To limit visual intrusion and impacts upon landscape character	Operation	Supplementary proprietary baffle devices could be fitted to luminaires along the perimeter of the site. The baffles would provide additional shielding of the light source.	No	Incorporation within OMP	Principal Operator
L4	To replace trees removed to facilitate the scheme	Reinstatement	Trees to be removed to facilitate the scheme shall be replanted on expiry of the consent (at the end of the operational phase). This planting should be replaced on a like-for-like basis supported by an appropriate planting specification produced by the Principal Operator.	No	Incorporation within Reinstatement Plan	Principal Operator and Reinstatement Contractor
Biodiversity (B)						
B1	To ensure no effects to protected species	Construction	<ul style="list-style-type: none"> All vegetation removal should be supervised by a suitably qualified ecologist and be undertaken outside of the main breeding bird season (and therefore, should be undertaken between September to February). 	No	Incorporation within CMP and Reinstatement Plan Ecological Clerk of Works on site	Principal Contractor and Ecological Clerk of Works

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
			<ul style="list-style-type: none"> In the event that any excavations are required, ensure that these are covered over night or that ramps are installed so that mammals do not become trapped. 			
Material Assets and Waste (M)						
M1	Ensure principals of the waste hierarchy are adhered to	Construction and Reinstatement	Where possible, ensure that the waste hierarchy is followed when dealing with waste on site: prevention, re-use and preparation for re-use, recycle, recovery, and disposal.	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor
M2	Ensure appropriate waste management during operation	Operation	Ensure waste bins are appropriately placed throughout the operational area.	No	Incorporation within the OMP	Principal Operator
Noise and Vibration (NV)						
NV1	Hours of working during construction	Construction and Reinstatement	All noisy operations would be completed between 08:00 and 18:00 on weekdays, and 08:00 to 13:00 on Saturdays, switching off noise-emitting equipment when not in use and the use of temporary noise barriers where appropriate. Where out of hours working is required, prior agreement would be sought with Dartford District Council.	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor
NV2	Limit noise emissions	Construction and Reinstatement	Implement the following noise mitigation measures: <ul style="list-style-type: none"> Ensure equipment is maintained, in good working order, and is used in accordance with the manufacturer's instructions. Fit equipment with silencers or mufflers. Manage deliveries to prevent queuing of site traffic. Do not leave plant running unnecessarily. Careful orientation of plant with directional features. Materials to be lowered instead of dropped from height. 	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
			<ul style="list-style-type: none"> Use of adjustable or directional audible vehicle-reversing alarms or use of alternative warning systems (for example, white noise alarms). Train and advise members of the construction team during toolbox talk briefings on quiet working methods. Erect temporary barriers to fully obscure the construction works from nearby receptors. 			
NV3	Limit noise emissions	Operation	Limit vehicle idling on-site as much as possible during operation.	No	Incorporation within the OMP	Principal Operator
Population and Health (PH)						
PH1	Reduce effects on local community	Construction and Reinstatement	<p>The following measures would reduce effects on the local community during operation:</p> <ul style="list-style-type: none"> Ensure local community are informed of the proposals. 	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor
PH2	Reduce effects on local community	Operation	<p>The following measures would reduce effects on the local community:</p> <ul style="list-style-type: none"> Ensure that HGV movements are controlled on site by marshalling. Ensure local community informed of the proposals. 	No	Incorporation within the OMP	Principal Operator
Road Drainage and the Water Environment (RDWE)						
RDWE1	To mitigate potential adverse effects upon RDWE	Construction and Reinstatement	<p>Construction activities must be managed in accordance with Construction Industry Research and Information Association (CIRIA) Guidelines. Guidance on best practice in relation to pollution prevention and water management is set out in the following documents:</p> <ul style="list-style-type: none"> CIRIA's Environmental good practice on-site. Environment Agency's Protect groundwater and prevent groundwater pollution. <p>Measures to be implemented to limit the impact of construction activities on the water environment include:</p>	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor and Reinstatement Contractor

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
			<ul style="list-style-type: none"> All construction workers to be briefed on the use of spill kits as part of the site induction. Any stockpiled materials to be stored within enclosed areas to enable the run-off to be stored and treated where required. All plant and machinery to be maintained in a good condition and any maintenance required would be undertaken within safe areas. Pollution prevention and spill response procedures to be developed by the contractor and a spill kit and clean up equipment maintained on-site. Dust suppression measures as described in AQ1 of this REAC. 			
RDWE2	To mitigate potential adverse effects upon RDWE	Operation	Undertake surveys of the existing drainage and carry out any repairs prior to the operation of the site. Ensure any damage to the drainage is repaired during operation of the scheme.	No	Incorporation within drainage design and OMP	Principal Contractor / Operator
RDWE3	To mitigate potential adverse effects upon RDWE	Operation	Inclusion of a pollution prevention plan in the OMP. Spill kits to be located across the site to be used in the event of a spill.	No	Incorporation within the OMP	Principal Contractor / Operator
Climate (C)						
C1	To reduce carbon emissions associated with the scheme	Construction and Reinstatement	<p>The carbon reduction principles as detailed within Section 3 of the Carbon Assessment and Reduction Report, Appendix I, would be considered including the following:</p> <ul style="list-style-type: none"> Transportation of materials to site would prioritise low-carbon modes where possible. Where possible, low-carbon construction materials and products would be preferred. 	No	Incorporation within CMP and Reinstatement Plan	Principal Contractor / Operator

Reference	Objective	Phase of development	Action (including specific location and any monitoring required)	Required to mitigate what would otherwise be a significant effect (Y / N)	Achievement criteria and reporting requirements (if applicable)	Responsible person(s)
			<ul style="list-style-type: none"> Where possible low-carbon construction plant and equipment would be used. Provision would be made to enable waste to be effectively segregated during construction, enabling materials to be effectively managed using the waste hierarchy, prioritising re-used and recycling over disposal. <p>Circular economy principles, such as Modern Methods of Construction, would be implemented, where possible.</p>			
C2	To reduce carbon emissions associated with the scheme	Operation	<p>The carbon reduction principles as detailed within Section 3 of the Carbon Assessment and Reduction Report, Appendix I, would be considered including the following:</p> <ul style="list-style-type: none"> Provision would be made to enable waste to be effectively segregated during operation, enabling materials to be effectively managed using the waste hierarchy, prioritising re-used and recycled over disposal. Where possible, measures would be put in place to limit profligate energy use by unintended user behaviours e.g. using motion sensors to control lights. <p>Where possible, measures would be put in place to limit profligate water use by unintended user behaviours e.g. using aerated taps.</p>	No	Incorporation within the OMP	Principal Operator
C3	To reduce carbon emissions associated with the scheme	Reinstatement	<p>Resource efficiency would be maximised through reinstatement. Opportunities for the re-use of assets following the end of operation would be explored as a priority. If re-use is not possible then recycling would be maximised.</p>	No	Incorporation within the OMP	Principal Operator and Reinstatement Contractor



Appendix D

Local Planning Authority Engagement

From:
Sent: 12 December 2022 09:41
To:
Subject: RE: Ebbsfleet Reinstatement Plans

From:
Sent: 08 June 2022 14:28
To: C **Subject:** RE: Ebbsfleet Reinstatement Plans

Hi,

I am happy with the reinstatement plan.

The only caveat I would have is that the landlord as in HS1 is satisfied with all works on completion to sign off on them.

If we can get this included within this document happy to sign off.

Kind Regards



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HS1 Limited, Registered in England: Company number 03539665.


From:
Sent: 08 June 2022 09:47
To: N
Subject: RE: Ebbsfleet Reinstatement Plans
Importance: High

[EXTERNAL email] Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email.

Welcome Back! Hope you've enjoyed the well-deserved break.

I assume your very busy playing catch up, but I'd appreciate if you could get back to me on the attached reinstatement plan and clarification on the western boundary access ASAP. We only have a couple of weeks now to get these agreed and signed off with DLUHC.

Kind Regards,

 HM Revenue & Customs Ground Floor, Barkley House, Castle Meadow Road Nottingham NG2 1BA ***Internal Post Only***

 HM Revenue & Customs S0842, Barkley House, Ground Floor Estates Transformation Property Central Mail Unit Newcastle NE98 1ZZ ***External Post Only***

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Estates: Have your say.

From:
Sent: 28 July 2022 15:34
To:
Subject: FW: Ebbsfleet Inland Border Facility - Reinstatement Plan
Attachments: HMRC Ebbsfleet IBF - Landlord Reinstatement Works 11.05.22.pdf

Ebbsfleet response also.

OFFICIAL

From:
Sent: 24 June 2022 08:55
To:
Cc:
Subject: RE: Ebbsfleet Inland Border Facility - Reinstatement Plan

Dear

Thanks for sharing the draft reinstatement plan (copy reattached) for comment. I provide a few comments below, which are made by EDC in its role as Local Planning Authority. I have spoken with Jen and understand she may provide separate comments on behalf of EDC as landowner.

My main comment relates to the proposed reinstatement of soft landscaping which is considered necessary so references to reinstatement of vegetation and trees is welcomed. However, it is considered necessary to include further details of the landscaping to be re-planted, including species and planting sizes, to provide certainty. I would expect this to align as closely as reasonably possible with what was removed – I believe EDC (as landowner) have a condition survey of the site pre-IBF works so would recommend that this information is used to inform the additional landscape detail requested.

I note that some newly installed infrastructure is proposed for retention, including the vehicular access from International Way and the surface water drainage penstock valve, but have no comments on this. It is noted that the CCTV network is proposed for removal but, subject to landowner/tenant views particularly around maintenance, I do wonder whether the relatively secluded nature of Car Park D might justify retention of that equipment for safety reasons.

As a general comment I think it would be helpful if an annotated site plan accompanied the written document for avoidance of any confusion, as well as confirmation of the timescale for completion of the reinstatement works.

Hope this is helpful, but please get in touch if you have any queries.

Kind regards



From:
Sent: 16 June 2022 17:09
To:
Cc:
Subject: RE: Ebbsfleet Inland Border Facility - Reinstatement Plan

Hi

Just to confirm receipt of your email and attachment. Mark's asked me to have a look, so I'll review ASAP and offer any comments next week. Trust that is OK with you.

Thanks



From:
Sent: 09 June 2022 11:37
To:
Cc:
Subject: RE: Ebbsfleet Inland Border Facility - Reinstatement Plan

Hi,

Further to below, I wanted to attach a draft reinstatement plan for any comments.

Very happy to discuss if easier.

Kind regards



Please note new mobile telephone number above

OFFICIAL

From:
Sent: 12 May 2022 18:08
To:
Subject: RE: Ebbsfleet Inland Border Facility - Reinstatement Plan

Thanks for the update.

From:
Sent: 12 May 2022 17:14
To:
Cc: J **Subject:** Ebbsfleet Inland Border Facility - Reinstatement Plan

Hi,

I hope you are well, it has been quite a while since we last spoke. I wanted to write regarding the above. You may recall that under our current temporary consent via the Special Development Order that we are required to submit a reinstatement plan for approval from DLUHC by the end of June 2022. This is being prepared in draft and we would like to share with you for any comments once available. The extent of works are being reviewed and we are keen to ensure we have engaged with EDC both as landowner and as planning authority prior to any submission.

More generally, we are also reviewing our ongoing requirements for our IBFs beyond 2022 and whether any uses would need to go beyond that. I will keep you updated on these discussions in relation to Ebbsfleet. You may be aware that we are in separate discussions with EDC colleagues regarding current lease arrangements related to this.

I will keep you updated on both matters and would like to share a draft reinstatement plan very shortly for any initial comments.

Kind regards

From:
Sent: 12 December 2022 09:28
To:
Subject: FW: Reinstatement of Ebbsfleet Inland Border Facility

From:
Sent: 12 December 2022 08:48
To:
Subject: FW: Reinstatement of Ebbsfleet Inland Border Facility
Morning

Please see feed back below. If is happy, I am happy

Thanks

From:
Sent: 09 December 2022 09:24
To:
Subject: RE: Reinstatement of Ebbsfleet Inland Border Facility

Hello,

Apologies for the delay in responding. I thought I had and then noticed it was in my Drafts.

Totally feel that the two options below and set out in 3.2 of the attached document are acceptable and happy for them to remain in situ and be retained.

Kind Regards



From:
Sent: 05 December 2022 11:50
To:
Subject: FW: Reinstatement of Ebbsfleet Inland Border Facility
Importance: High

Morning

Please see below and attached. Can you cast your eye over this and check you are happy and let me know so that I can respond accordingly

Thanks

T

From:
Sent: 05 December 2022 10:14
To:
Cc:
Subject: Reinstatement of Ebbsfleet Inland Border Facility
Importance: High

Hi,

I hope you are well. I wanted to contact you on the above. Part of the red line boundary of the scheme included highway in KCC's ownership. As part of the reinstatement proposals, we need to demonstrate that we have engaged with all landowners and have had received any comments etc, hence this email.

I attach a copy of the plan but in summary, it is proposed to revert the site back to its previous use save for a couple of elements to be retained as set out below:

Access from International Way

A number of modifications were carried out to the access and egress from the site to facilitate IBF site operations. A new entrance for HGV's was created along the Western boundary of the site off International Way including creating an opening in the boundary and adaptations to the highway and pedestrian footpath. To improve access into the car park once reinstated, the landlord has requested that this item be retained and not reinstated. The line markings on the immediate highway network would be removed to allow two lane traffic around International Way.

3.2.2 Automated Penstock Valves

As a condition of the original SDO, penstock valves were fitted to the existing surface water drainage system where it exits the site at two locations along the Eastern boundary. To provide ongoing pollution control of

the site when it is reinstated back into its use as a car park, in agreement with the landlord it is proposed to retain these two valves in their existing locations.

Do you have any comments at all and/or would you mind confirming that KCC has no concerns with the reinstatement proposals (if that is the case)? We are hoping to submit the plan asap this week so very grateful if you would be able to respond as soon as convenient if at all possible.

Many thanks

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